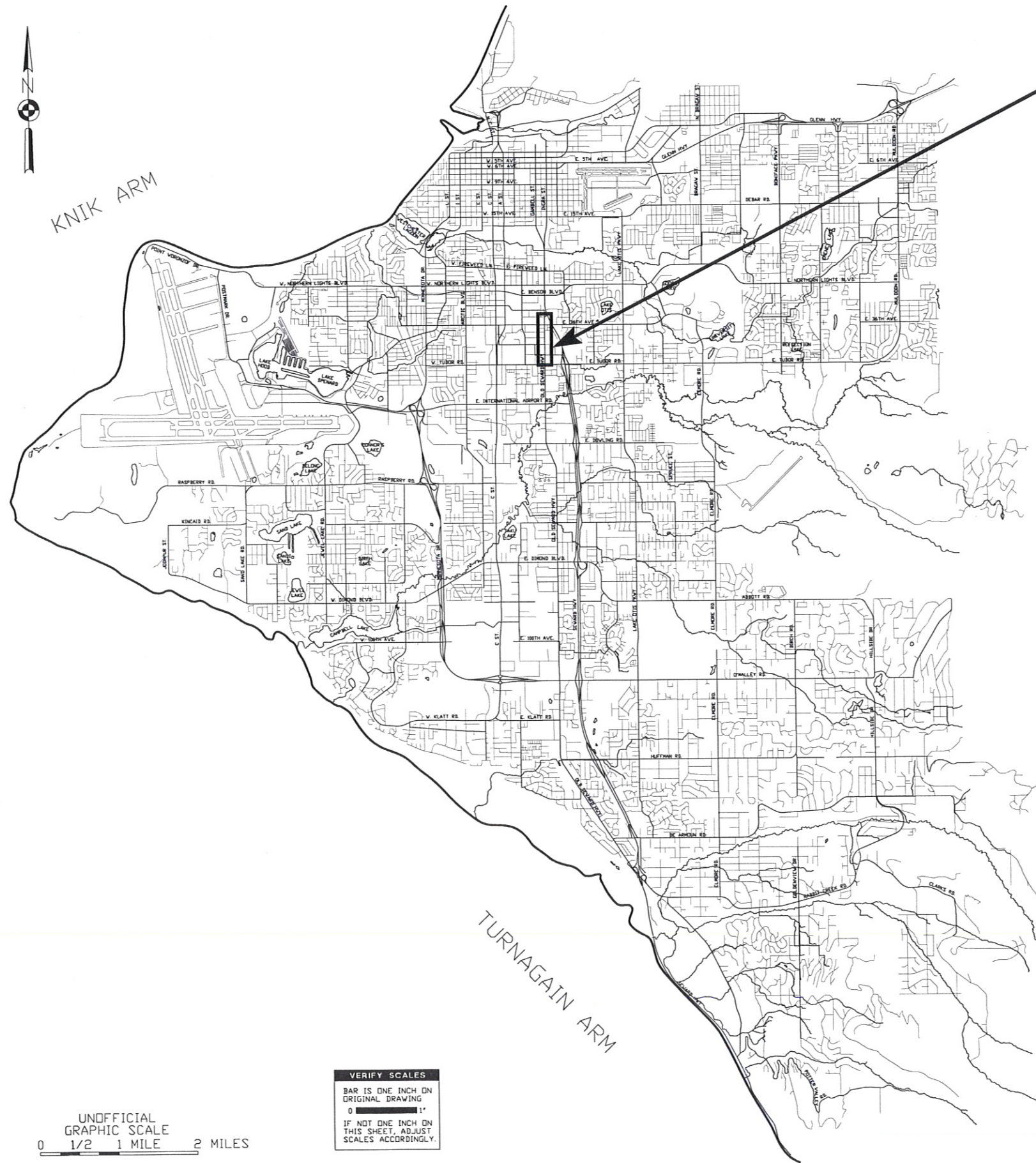




KNIK ARM



PROJECT AREA
THIS CONTRACT



MUNICIPALITY OF ANCHORAGE
PUBLIC WORKS DEPARTMENT
PROJECT MANAGEMENT AND ENGINEERING DIVISION

OLD SEWARD HIGHWAY
SURFACE REHABILITATION

34TH AVENUE TO TUDOR ROAD
10-13

APPROVED BY:

DANIEL A. SULLIVAN
MAYOR

STEPHEN D. SHRADER, P.E.
ENGINEERING MANAGER

VERIFY SCALES
BAR IS ONE INCH ON
ORIGINAL DRAWING
0 1'
IF NOT ONE INCH ON
THIS SHEET, ADJUST
SCALES ACCORDINGLY.

UNOFFICIAL
GRAPHIC SCALE
0 1/2 1 MILE 2 MILES

GENERAL NOTES

- CONTRACTOR SHALL COMPLETE CONSTRUCTION IN ACCORDANCE WITH THE MUNICIPALITY OF ANCHORAGE STANDARD SPECIFICATIONS, DATED 2009, REVISION 2, HEREAFTER REFERRED TO AS M.A.S.S., AS AMENDED BY THE SPECIAL PROVISIONS.
- WITH THE EXCEPTION OF THE PORTION OF THE OSH FROM 40TH AVE TO 42ND AVE, A COMPLETE SURVEY WAS NOT PERFORMED FOR THE REMAINDER OF THE PROJECT. STATIONING IS APPROXIMATE AND ASSIGNED TO THE CENTER OF THE RIGHT-OF-WAY AND EXTENDS FROM CENTERLINE OF RIGHT-OF-WAY INTERSECTIONS. THE CENTERLINE OF PAVEMENT DOES NOT NECESSARILY MATCH THE CENTERLINE OF RIGHT-OF-WAY.
- LOCATIONS DEPICTED FOR THE UTILITIES AND OTHER EXISTING FEATURES ARE APPROXIMATE. GAS, ELECTRIC, TELEPHONE, AND CABLE MAY NOT BE DEPICTED IN ALL LOCATIONS. CONTRACTOR SHALL LOCATE AND VERIFY ALL UTILITY LOCATIONS AND DEPTHS (WHERE APPROPRIATE) PRIOR TO CONSTRUCTION. CONTRACTOR SHALL LOCATE ALL LIDS/COVERS/ETC. THAT PROTRUDE INTO THE PAVEMENT, IN THE FIELD PRIOR TO BEGINNING RESURFACING WORK.
- CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS PRIOR TO BEGINNING CONSTRUCTION. THE PERMITS SHALL BE MAINTAINED AT THE JOB SITE.
- STATIONING IS R.O.W. CENTERLINE, UNLESS OTHERWISE NOTED. DIMENSIONS AND ELEVATIONS ARE TO TOP BACK OF CURB (TBC), UNLESS NOTED OTHERWISE.
- CONTRACTOR SHALL MAINTAIN "REDLINE" RECORD DRAWINGS ON A CLEAN SET OF CONSTRUCTION DRAWINGS IN ACCORDANCE WITH M.A.S.S. DIVISION 65 CONSTRUCTION SPECIFICATIONS FOR CONSTRUCTION SURVEY. THE CONTRACTOR SHALL MAINTAIN THE "REDLINES" CURRENT ON A DAILY BASIS AND SHALL BE AVAILABLE TO THE ENGINEER FOR INSPECTION ON THE JOBSITE.
- CONTRACTOR SHALL RECORD SURVEY NOTES AND SUBMIT DAILY TO THE ENGINEER. CONTRACTOR SHALL RECORD SURVEY NOTES FOR SUBMITTAL WITH AS-BUILT PLANS, INCLUDING HORIZONTAL AND VERTICAL LOCATIONS OF ALL UTILITIES ENCOUNTERED IN THE FIELD. CONTRACTOR SHALL RECORD ALL DEVIATIONS FROM THE PLANS.
- CONSTRUCTION OPERATIONS REQUIRED FOR THIS PROJECT SHALL REMAIN WITHIN EXISTING M.O.A. AND ADOT/PF RIGHTS-OF-WAY AND EASEMENTS, UNLESS OTHERWISE APPROVED IN WRITING BY THE ENGINEER AND THE AFFECTED PROPERTY OWNER(S).
- SHORING OF UTILITY POLES IS INCIDENTAL TO THE PROJECT AND NO SEPARATE PAYMENT SHALL BE MADE.
- UNDERGROUND AND OVERHEAD ELECTRICAL AND TELECOMMUNICATION LINES AND POLES OCCUR WITHIN THE PROJECT AREA; CONTRACTOR SHALL COORDINATE WORK ACCORDINGLY. ALL WORK IN CLOSE PROXIMITY TO EXISTING OVERHEAD AND UNDERGROUND LINES AND POLES SHALL COMPLY WITH APPLICABLE FEDERAL, STATE, AND LOCAL STATUTES, CODES AND GUIDELINES, AND THE ELECTRICAL FACILITY CLEARANCE REQUIREMENTS OF THE GOVERNING UTILITY. HAND DIGGING IS REQUIRED WITHIN TWO FEET OF BURIED ELECTRICAL CABLE.
- CONTRACTOR SHALL SAWCUT EXISTING PAVEMENT (ROADS, PARKING AREAS, DRIVEWAYS, ETC..) TO A LINE TWO (2) FEET BEYOND THE PROPOSED IMPROVEMENTS, AND MORE IF NECESSARY, DURING THE INITIAL EXCAVATION OPERATIONS. IF EXISTING PAVEMENT HAS BEEN LIFTED, IF EDGE DOES NOT OCCUR IN UNDISTURBED MATERIAL, OR IF EDGE IS LOCATED WITHIN A TRAVEL LANE, FURTHER REMOVAL MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER, TO PROVIDE A PROPER TRANSITION BETWEEN NEW AND EXISTING PAVEMENT. SAW CUTTING OF EXISTING PAVEMENT IS INCIDENTAL TO THE BID ITEM "REMOVE EXISTING PAVEMENT", AND NO SEPARATE PAYMENT SHALL BE MADE.
- CONTRACTOR SHALL APPLY TACK COAT TO THE SAW CUT ASPHALT FACE PRIOR TO PAVING. TACK COAT FOR THIS WORK IS INCIDENTAL TO THE PROJECT AND NO SEPARATE PAYMENT SHALL BE MADE.
- CONTRACTOR SHALL SAWCUT CURB & GUTTER AND SIDEWALK AT THE NEAREST JOINT AT OR BEYOND REMOVAL LIMITS OR AS DIRECTED BY THE ENGINEER. SAWCUTTING IS INCIDENTAL TO THE RESPECTIVE BID ITEM.
- CONTRACTOR SHALL NOT ORDER CONCRETE TRUCKS FOR CURB RAMPS BEFORE VERIFYING CURB RAMP LAYOUT WITH ENGINEER.
- CONTRACTOR SHALL DELIVER REMOVED SIGN FACES TO THE MOA TRAFFIC PAINT AND SIGN SHOP. SIGN REMOVAL AND DELIVERY IS INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
- CONTRACTOR SHALL MAINTAIN STOP AND STREET NAME SIGNS OPERATIONAL IN THE PROJECT AREA DURING CONSTRUCTION.
- LIMITS OF EXCAVATION AND BACKFILL ARE SHOWN ON THE DRAWINGS OR AS DIRECTED BY THE ENGINEER.
- CONTRACTOR SHALL REMOVE ORGANIC MATERIAL FROM THE SUBGRADE AND NOT USE AS BACKFILL.
- WORK AND MATERIALS REQUIRED FOR REMOVING LITTER, DEBRIS, AND OTHER MATERIALS AS DIRECTED BY THE ENGINEER THAT EXISTS WITHIN THE PROJECT LIMITS IS INCIDENTAL TO THE PROJECT AND NO SEPARATE PAYMENT SHALL BE MADE.
- CONTRACTOR SHALL REPLACE ALL DISTURBED PROPERTY CORNERS IN ACCORDANCE WITH M.A.S.S. SECTION 65.02 CONSTRUCTION SURVEYING, ARTICLE 2.1 PROJECT CONTROL. PAYMENT FOR REPLACING PROPERTY CORNERS IS INCIDENTAL TO THE PROJECT AND NO SEPARATE PAYMENT SHALL BE MADE.
- CONTRACTOR SHALL PLACE TOPSOIL (4" DEPTH) AND SEED DISTURBED AREAS. SEED SCHEDULE AND MIX SHALL CONFORM TO M.A.S.S. AND BE APPROVED BY THE ENGINEER.


- CONTRACTOR SHALL RESTORE DISTURBED PROPERTY TO PRECONSTRUCTION CONDITION(S), UNLESS OTHERWISE DIRECTED BY THE ENGINEER. PAYMENT FOR RESTORING DISTURBED PROPERTY IS INCIDENTAL TO THE PROJECT AND NO SEPARATE PAYMENT SHALL BE MADE.
- PREVENT SILT AND SEDIMENT RUNOFF BY SITE CONTAINMENT WITH SILT FENCE OR OTHER APPROPRIATE BEST MANAGEMENT PRACTICE. PAYMENT IS INCIDENTAL TO M.A.S.S. SECTION 20.02 STORMWATER POLLUTION PREVENTION PLAN.
- CONTRACTOR MAY NOT PUMP OR OTHERWISE DIVERT WATER RESULTING FROM CONTRACTOR'S DEWATERING EFFORT OR SURFACE RUNOFF INTO EXISTING STORM DRAINS, UNLESS REQUIRED PERMITS, INCLUDING, BUT NOT LIMITED TO THE ALASKA DEPARTMENT OF ENVIRONMENTAL CONSERVATION (A.D.E.C.), ARE OBTAINED BY CONTRACTOR. UNDER NO CIRCUMSTANCES IS THE CONTRACTOR ALLOWED TO DIVERT WATER FROM THE EXCAVATION ONTO THE ROADWAYS. CONTRACTOR SHALL PROVIDE DISPOSAL SITE FOR EXCESS WATER AND IS RESPONSIBLE FOR SECURING ALL NECESSARY PERMITS AND APPROVALS. CONTRACTOR SHALL PROVIDE COPIES OF PERMITS AND APPROVALS TO THE ENGINEER.
- ADJUSTING SURVEY MONUMENTS TO FINISH GRADE IS INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
- CONTRACTOR SHALL REMOVE OVERGROWTH (GRASS, ROOTS, ROOT MAT, AND SOIL) ON BACK OF SIDEWALKS TO EXPOSE ENTIRE SIDEWALK I.A.W. 20.04 AND AS DIRECTED BY THE ENGINEER. WORK SHALL BE PAID FOR UNDER 20.04 "CLEARING AND GRUBBING OF SIDEWALKS" AND NO ADDITIONAL PAYMENT SHALL BE MADE.

STORM DRAIN NOTES

- CONTRACTOR WILL ASCERTAIN DRAINAGE PATTERNS AT CURB RETURNS AND ENSURE THAT EXISTING PATTERNS ARE MAINTAINED. MINIMUM FLOW LINE SLOPE IN CURB RETURN IS 0.5% OR EXISTING SLOPE, WHICHEVER IS LESS.
- CONTRACTOR SHALL GRADE SURFACES TO PROVIDE POSITIVE DRAINAGE IN AND AROUND RAMPS. SURFACE GRADING IS INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
- CONTRACTOR SHALL DELIVER REMOVED GRATES, FRAMES, SALVAGEABLE MANHOLES AND CATCH BASINS, GRADE RINGS, CONES, LADDERS, AND OTHER ITEMS AS DETERMINED BY THE ENGINEER TO THE MOA KLOEP STATION MAINTENANCE YARD AT 5701 NORTHWOOD DRIVE. CONTRACTOR SHALL CALL STREET MAINTENANCE AT 343-8277 DURING NORMAL BUSINESS HOURS TO COORDINATE DELIVERY. THIS WORK IS INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT WILL BE MADE.
- CONTRACTOR SHALL F & I INSULATION BOARD (R-20) BETWEEN THE STORM DRAIN IMPROVEMENTS AND THE WATER & SANITARY SEWER UTILITIES, WHEN HORIZONTAL OR VERTICAL CLEARANCE IS LESS THAN THREE (3) FEET, FROM OUTSIDE OF PIPE (OR MANHOLE) TO OUTSIDE OF PIPE (OR MANHOLE); HORIZONTAL OR VERTICAL SEPARATION BETWEEN STORM DRAIN AND WATER LINES SHALL NOT BE LESS THAN 18". INSTALL INSULATION I.A.W. M.A.S.S. STD. DTL. 20-9. THIS WORK IS INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT WILL BE MADE.
- PLACE STORM DRAIN PIPE JOINTS AT LEAST 9 FEET FROM WATERLINE CROSSING(S).

ABBREVIATIONS

ADA	AMERICANS WITH DISABILITIES ACT OF 1990
ADOT/PF	STATE OF ALASKA DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES
A.S.A.P.	AS STEEP AS PRACTICAL
B.O.P.	BEGINNING OF PROJECT / BOTTOM OF PIPE
BM	BENCH MARK
C&G	CURB & GUTTER
CB	CATCH BASIN
CBMH	CATCH BASIN MANHOLE
CC	CURB CUT
CL	CENTERLINE
CIP	CURED-IN-PLACE
CMP	CORRUGATED METAL PIPE
CONST.	CONSTRUCT
CPEP	CORRUGATED POLYETHYLENE PIPE
CU	COPPER
DIP	DUCTILE IRON PIPE
DTL	DETAIL
DW	DRIVEWAY
DW CC	DRIVEWAY CURB CUT
E.O.P.	END OF PROJECT / EDGE OF PAVEMENT
E.C.	FRONT EDGE OF CURB
ELEV.	ELEVATION
ESMT.	EASEMENT
EXIST.	EXISTING
F&I	FURNISH AND INSTALL
F.C.	FACE OF CURB
F.G.	FINISHED GRADE
F&G	FRAME AND GRATE
FL	FLOW LINE
G.B.	GRADE BREAK
HDPEP	HIGH DENSITY POLYETHYLENE PIPE
I.A.W.	IN ACCORDANCE WITH
INV.	INVERT
L.C.	LEVELING COURSE
LOC	LOCATION
LT	LEFT
M.A.S.S.	MUNICIPALITY OF ANCHORAGE STANDARD SPECIFICATIONS, 2009, REVISION 2
MAX.	MAXIMUM
M.E.	MATCH EXISTING
MH	MANHOLE
MIN.	MINIMUM
MON	MONUMENT
MSL	MEAN SEA LEVEL
N.T.S.	NOT TO SCALE
NWT	NO WATER TABLE ENCOUNTERED
OGS	OIL AND GRIT SEPARATOR
P.C.C.	PORTLAND CEMENT CONCRETE
P.C.	POINT OF CURVATURE
PCMP	PRECOATED CORRUGATED METAL PIPE
PL	PROPERTY LINE
P.U.E.	PUBLIC USE EASEMENT
P.R.C.	POINT OF REVERSE CURVATURE
P.T.	POINT OF TANGENCY
PVC	POINT OF VERTICAL CURVE
PVI	POINT OF VERTICAL INTERSECTION
PVMT.	A.C. PAVEMENT
PVT	POINT OF VERTICAL TANGENCY
R	RADIUS
R&R	REMOVE AND REPLACE
R.A.P.	RECYCLED ASPHALT PAVEMENT
R.O.W.	RIGHT-OF-WAY
RT	RIGHT
SDFI	STORM DRAIN FIELD INLET
SDMH	STORM DRAIN MANHOLE
S.I.	STREET INTERSECTION
S.P.	SPECIAL PROVISION
SS	SANITARY SEWER
SSMH	SANITARY SEWER MANHOLE
STA	STATION
STD. DTL.	STANDARD DETAIL FOUND IN M.A.S.S.
ST	STREET
STR	STRUCTURE
S/W	SIDEWALK
TBC	TOP BACK OF CURB
T.C.E.	TEMPORARY CONSTRUCTION EASEMENT
T.C.P.	TEMPORARY CONSTRUCTION PERMIT
V.B.	WATER VALVE BOX
VC	VERTICAL CURVE
V.G.	VALLEY GUTTER
(30')	DIMENSION FROM RECORD DRAWINGS

 DETAIL AND SHEET NUMBER FOR DETAIL

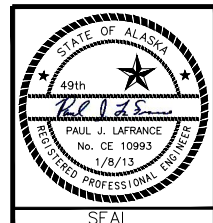
DRAWING INDEX

SHEET NO.	DRAWING
<u>GENERAL</u>	
G1	COVER SHEET
G2	NOTES, ABBREVIATIONS AND DRAWING INDEX
G3	KEY MAP AND LEGEND
G4	CONTROL SURVEY
G5	CONTROL SURVEY
G6	RIGHT-OF-WAY MAP
G7	RIGHT-OF-WAY MAP
G8	EASEMENT & PERMIT INDEX MAP
G9	EASEMENT & PERMIT INDEX MAP
<u>DETAILS</u>	
D1	TYPICAL SECTIONS & DETAILS
D2	ADA WORK & UNIDIRECTIONAL CURB RAMP DETAIL
D3	DRIVEWAY ADA RAMPS
<u>STREET & STORM</u>	
P1	OLD SEWARD HIGHWAY STREET & STORM IMPROVEMENTS
P2	OLD SEWARD HIGHWAY STREET & STORM IMPROVEMENTS
P3	E 42ND AVENUE STREET & STORM IMPROVEMENTS
P4	E 40TH AVENUE STREET & STORM IMPROVEMENTS
P5	OLD SEWARD HIGHWAY STREET & STORM IMPROVEMENTS
<u>STRIPING</u>	
S1	STRIPING PLAN
S2	STRIPING PLAN
<u>SIGNING, ILLUMINATION, AND BUS STOP WORK</u>	
T1	SIGNING AND ILLUMINATION
T2	SIGNING AND ILLUMINATION
T3	SIGNING AND ILLUMINATION
T4	SIGNING AND ILLUMINATION
T5	SIGNING AND ILLUMINATION
T6	SIGNING AND ILLUMINATION
T7	HOLIDAY BUS STOP
T8	42ND AVE BUS STOP
T9	40TH AVENUE SIGNAL IMPROVEMENTS
T10	SIGN SUMMARY
T11	SIGN SUMMARY
T12	DETAIL
T13	BUS STOP ELECTROLIER DETAILS
T14	LOAD CENTER SUMMARY

CALL BEFORE YOU DIG (811)

THE CONTRACTOR SHALL NOTIFY ALL AREA UTILITY COMPANIES PRIOR TO COMMENCEMENT OF EXCAVATION. THE FOLLOWING IS A PARTIAL LIST:
LOCATE CALL CENTER OF ALASKA 278-3121
 (INCLUDES ACS, AWWU, CEA, ENG, BUTLER AVIATION/TESORO, GCI CABLE, MLP, TRAFFIC SIGNALS, MOA STORM/STREETS, AND ALASKA FIBER STAR.)
STATE STORM/STREET LIGHTS 333-2411
MILITARY FUEL LINES 552-3760
AK RAILROAD 265-2520

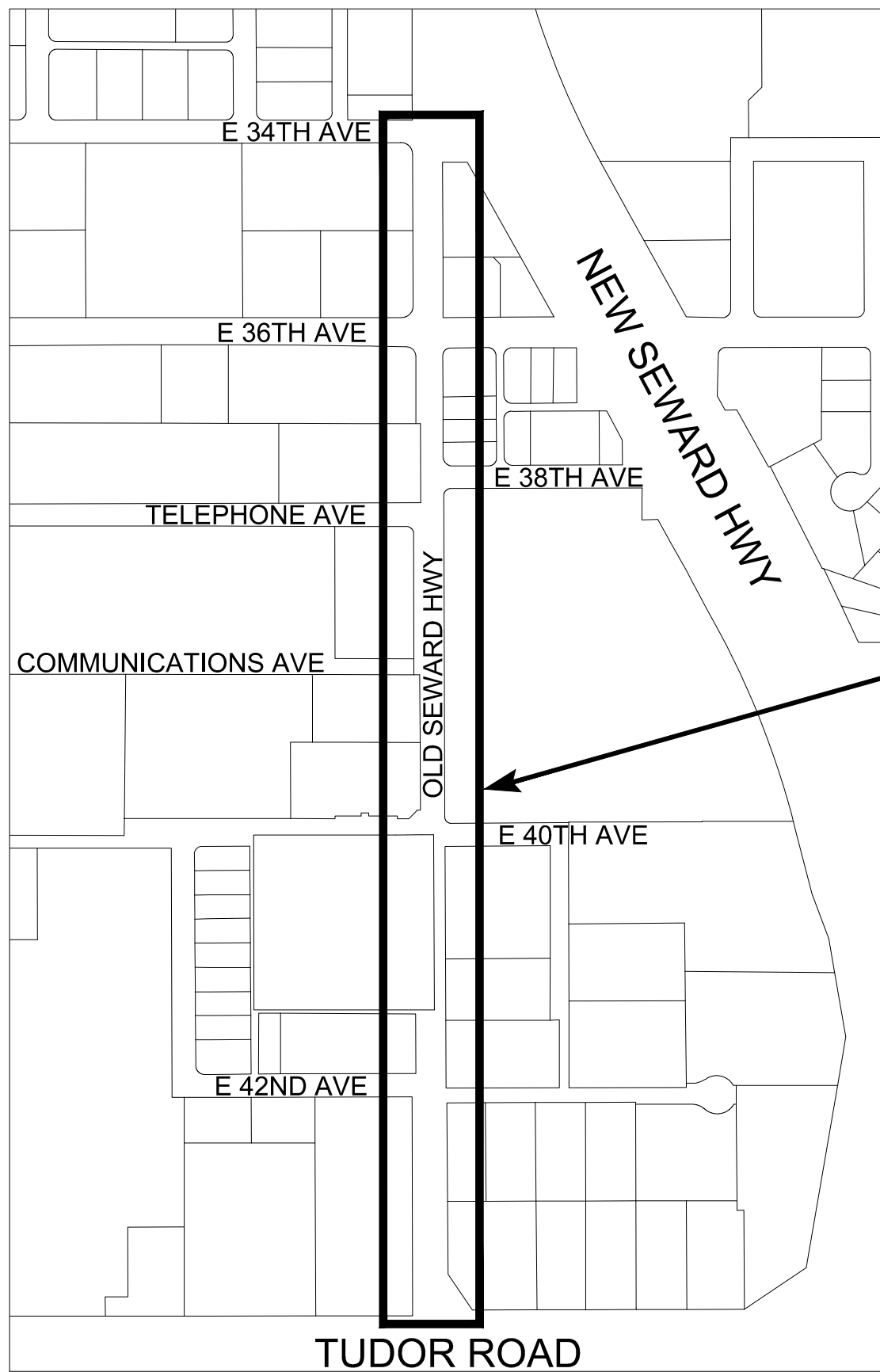
G:\Public Works\PM\10-13 Old Seward Hwy Surface Rehab - 34th Ave to Tudor Rd\Design\ENG\dwg\10-13 Notes.dwg



PUBLIC WORKS DEPARTMENT			
10-13	OLD SEWARD HIGHWAY SURFACE REHABILITATION	SCH. A-D	
NOTES, ABBREVIATIONS AND DRAWING INDEX			
34TH AVENUE TO TUDOR ROAD			
SCALE: N.T.S.	DATE: JAN 2013	GRID: SW1631, SW1632, SW1731, SW1732	SHEET G2 of G9
ACCT. NO.			

G:\Public Works\PM\PA\10-13 Old Seward Hwy Surface Rehab - 34th Ave to Tudor Rd\Design\ENG\dwg\10-13 KM Legend.dwg

KEY MAP

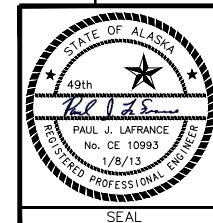


PROJECT AREA

PLAN

EXISTING	PROPOSED	EXISTING	PROPOSED
			FIRE HYDRANT
			STREET SIGNS
			SANITARY SEWER MANHOLE
			STORM DRAIN MANHOLE
			CATCH BASIN
			CATCH BASIN FIELD DRAIN
			CATCH BASIN MANHOLE
			CLEANOUT
			MANHOLE - TELEPHONE OR ELECTRICAL
			SANITARY SEWER SERVICE CLEANOUT
			GUY WIRE
			KEY BOX/WATER VALVE
			CULVERT
			BUILDING FINISH FLOOR
			BUILDING
			PAVEMENT ROTOMILLING
			P.C.C. CONCRETE
			REMOVE & REPLACE P.C.C. CONCRETE
			REMOVE & REPLACE PAVEMENT
			PAVEMENT
			GRAVEL
			VALLEY GUTTER
			AC SWALE
			DRAINAGE ARROW
			BOLLARD
			PERPENDICULAR CURB RAMP
			PARALLEL CURB RAMP
			UNIDIRECTIONAL CURB RAMP
			CABLE TELEVISION PEDESTAL
			FIBER OPTIC VAULT
			GAS METER

PROFILE		
EXISTING	PROPOSED	
		EXISTING GROUND CENTERLINE
		EXISTING GROUND 30' LEFT
		EXISTING GROUND 30' RIGHT
		PIPE
		CURED-IN-PLACE PIPE LINING
		GRADE AT C OF PAVEMENT
		SANITARY SEWER LINE AND MANHOLE
		STORM DRAIN OR SUBDRAIN LINE AND MANHOLE
		UTILITY LINE CROSSING



PUBLIC WORKS DEPARTMENT

10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH. A-D

KEY MAP AND LEGEND

34TH AVENUE TO TUDOR ROAD

SCALE: 1"=200'

DATE: JAN 2013 GRID: SW1631, SW1632, SW1731, SW1732 SHEET **G3** of **G9**

DENALI ST.

FAIRBANKS ST.

E. TUDOR RD.

E. TUDOR RD.

PLAT No. 82-274
TUDOR PARK SUBD.
BLK. 1

COMMERCIAL PARK
SUBD. BLK. 1
PLAT No. 72-16

PLAT No. 84-368
TUDOR SQUARE SUBD.
BLK. 1

COMMERCIAL
PARK
SUBD.
BLK. 1
PLAT
No.
78-165

UNSUBDIVIDED
QUIT-CLAIM DEED
BK. 3019, PG. 429

TRACT 1
COMMUNICATIONS SUBD.
PLAT No. 2010-99

TUDOR CORNER
SUBD.
PLAT No. 67-4

PREFERRED
INVESTMENT
COMPANY SUBD.
PLAT No. 66-23

FORTIETH STREET
VENTURE SUBD.
PLAT No. 82-504

CENTRAL CITY SUBD.
PLAT No. 85-76
TRACT A-2

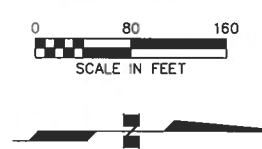
PLAT No. 69-5
1
8B
PLAT No. 69-5
9B

PLAT No. 69-5
7A
TUDOR CORNER
SUBD.
PLAT No. 95-26
8C

PREFERRED
INVESTMENT
COMPANY
SUBD. NO. 2
PLAT No. 70-30D
1B
PLAT No. 83-96
FRAGMENT LOT 3
PLAT No. 85-107

PLAT No. 85-107
FRAGMENT LOT 2

PLAT No. 85-107
FRAGMENT LOT 1



OLD SEWARD HWY. ALIGNMENT

PI Station	Northing	Easting	Direction	Distance
53+65.74	326,953.34	351,354.97		
56+31.34	327,218.93	351,353.98	N 0° 12' 50" W	265.60
59+00.00	327,487.59	351,352.96	N 0° 13' 09" W	268.66
60+00.00	327,587.59	351,352.58	N 0° 12' 59" W	100.00
66+84.93	328,272.51	351,349.95	N 0° 13' 11" W	684.93
72+00.00	328,787.58	351,348.32	N 0° 10' 52" W	515.07
75+27.58	329,115.16	351,347.10	N 0° 12' 50" W	327.58
76+25.81	329,213.39	351,346.73	N 0° 12' 51" W	98.23
77+85.13	329,372.71	351,346.14	N 0° 12' 49" W	159.33
80+05.65	329,593.23	351,345.32	N 0° 12' 50" W	220.51
85+36.40	330,123.98	351,343.34	N 0° 12' 48" W	530.76
89+26.04	330,513.62	351,341.89	N 0° 12' 48" W	389.64

NOTE: SEE CONTROL SHEET No. 2 FOR OSH STATION & OFFSET INFORMATION.

E. 40th AVE.

No.	Northing	Easting	Station	Offset	Description
27	328726.6298	351348.3960	39+98.43	-454.12	Found Aluminum Cap
51	328272.2963	350689.7222	33+39.77	0.15	Found Aluminum Cap
103	328272.5145	351349.9523	40+00.00	0.00	Found Aluminum Cap
104	327587.5915	351352.5727	40+00.93	684.93	Found Aluminum Cap
107	328273.3293	351679.9656	43+30.01	0.00	Found Rebar
124	328340.5625	351307.7252	39+57.77	-68.05	Found Rebar
125	328317.5614	351284.8078	39+34.85	-45.04	Found Rebar
126	328317.5523	351255.2396	39+05.28	-45.03	Found Rebar
127	328324.5062	351255.2319	39+05.28	-51.99	Found Rebar

E. 42nd AVE.

No.	Northing	Easting	Station	Offset	Description
42	326952.3764	351354.8897	16+61.94	660.22	Found Aluminum Cap Monument
58	327582.8231	351292.4239	16+00.11	29.71	Found Plastic Cap on Rebar
103	328272.5145	351349.9523	16+58.34	-659.93	Found Aluminum Cap
104	327587.5915	351352.5727	16+60.27	25.00	Found Aluminum Cap
112	327582.4353	351039.3350	13+47.02	29.84	Found Rebar
113	327611.9218	350692.2811	10+00.00	0.00	Found Aluminum Cap
114	327581.9176	350692.3456	10+00.03	30.00	Found Aluminum Cap Monument
115	327642.1808	350888.9135	11+96.66	-30.06	Found Rebar
116	327642.1967	351019.3249	13+27.07	-29.94	Found Rebar
117	327641.9852	350947.5638	12+55.31	-29.80	Found Rebar
121	327563.6871	351881.5248	21+89.19	49.44	Found Plastic Cap on Rebar
122	327563.9541	352029.9253	23+37.59	49.32	Found Plastic Cap on Rebar
123	327551.5210	352062.9374	23+70.59	61.79	Found Plastic Cap on Rebar

FIELD BOOKS	BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REV	DATE	DESCRIPTION	BY	REV	DATE	DESCRIPTION	BY	
DESIGN: MOA Book 3576, Pages 1 - 73	GAAB 93	MOA 1988 Benchmark Book, D-54	113.95'	BASE	R&M		TELEPHONE											
MOA Book 3589, Pages 1 - 55				TOPOGRAPHY			ELECTRIC											
STAKING:				PROFILE			CABLE TV											
ASBUILT:				SANITARY SEWER			TRAFFIC SIGNAL											
CONTRACTOR:				STORM SEWER			DESIGN											
INSPECTOR:				WATER			QUANTITIES											
CONSTRUCTION RECORD				GAS			MUN. FINAL CHECK											
				PLAN CHECK														

RSM
RSM CONSULTANTS, INC.
8101 Vanguard Drive
Anchorage, Alaska 99507
907 588 1707 voice
907 588 3404 fax
www.rsmconsult.com

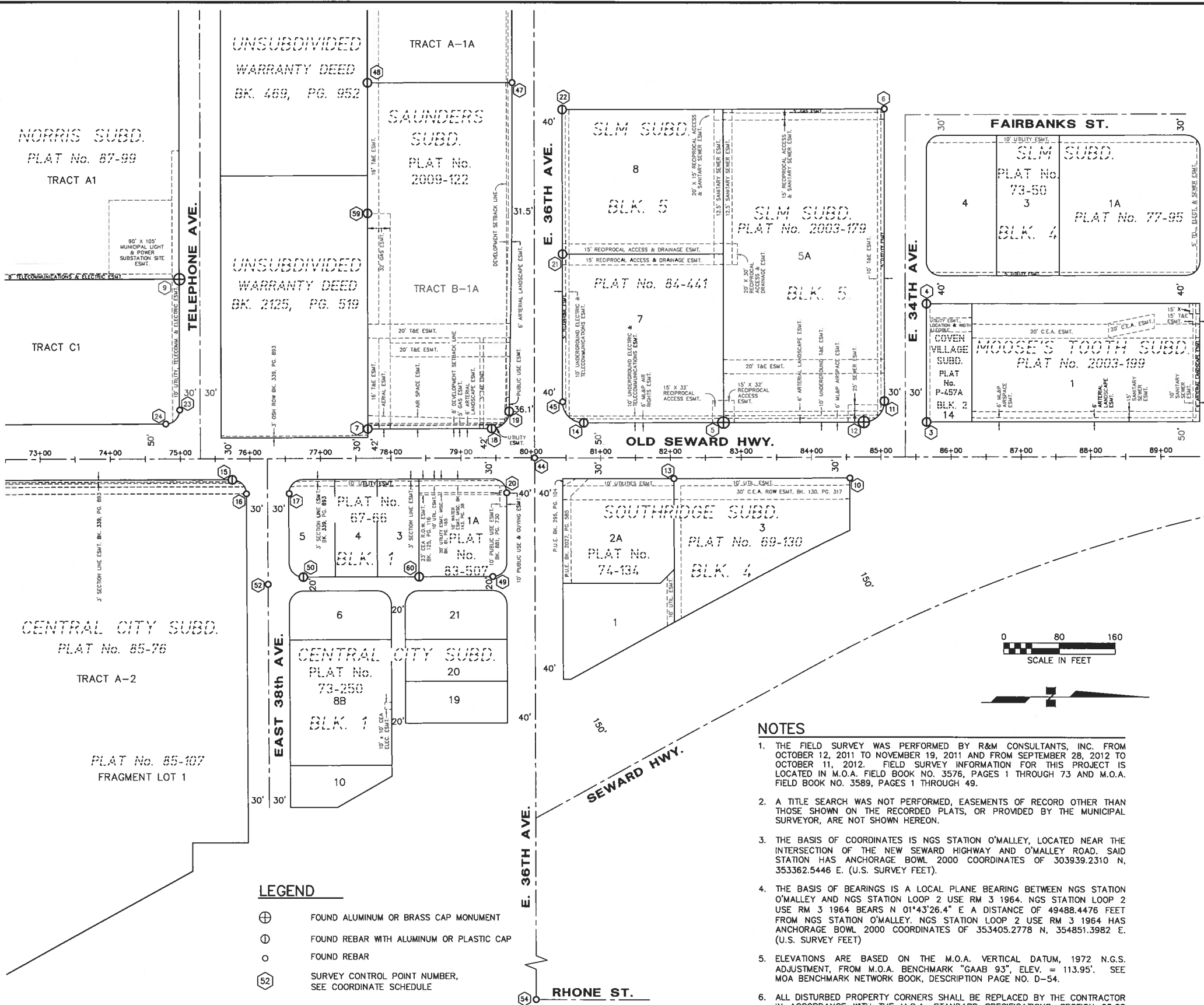
STATE OF ALASKA
Professional Seal
William D. Cohen
No. LS-7537
1/12/2013
REGISTERED PROFESSIONAL LAND SURVEYOR



PUBLIC WORKS DEPARTMENT
10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH. A-D
CONTROL SURVEY
34TH AVENUE TO TUDOR ROAD
HORZ SCALE: 1"=80' DATE: JAN. 2013 GRID SW1731, SW1732
VERT SCALE: N/A ACCT. NO. SHEET G4 of G9

OLD SEWARD HWY.

No.	Northing	Easting	Station	Offset	Description
3	330152.7532	351293.1930	85+65.36	-50.04	Found Plastic Cap on Rebar
4	330152.6142	351124.0956	85+65.85	-219.14	Found Plastic Cap on Rebar
5	329862.6146	351294.2901	82+75.22	-50.02	Found Aluminum Cap Monument
6	330092.7087	350846.5223	85+06.98	-496.93	Found Rebar
7	329354.4962	351304.1252	77+67.07	-42.08	Found Plastic Cap on Rebar
8	328696.5845	351298.4856	71+09.16	-50.13	Found Aluminum Cap Monument
9	329086.4330	351090.0119	74+99.81	-257.20	Found Aluminum Cap Monument
10	330042.5532	351373.6348	84+54.86	29.99	Found Rebar
11	330092.4956	351263.5168	85+05.22	-79.94	Found Plastic Cap on Rebar
12	330062.6604	351293.5050	84+75.27	-50.06	Found Aluminum Cap Monument
13	329792.6120	351374.6039	82+04.92	30.03	Found Rebar
14	329662.9518	351294.9472	80+75.56	-50.11	Found Plastic Cap on Rebar
15	329162.2293	351376.7941	75+74.53	29.87	Found Aluminum Cap
16	329182.4875	351396.8213	75+94.72	49.97	Found Rebar
17	329242.0381	351396.5343	76+55.27	49.91	Found Rebar
18	329531.5105	351303.5071	79+44.09	-42.04	Found Plastic Cap on Rebar
19	329556.4132	351278.9105	79+69.08	-66.54	Found Plastic Cap on Rebar
20	329552.6598	351395.3901	79+64.89	49.92	Found Rebar
21	329632.6905	351054.0850	80+46.20	-291.08	Found Plastic Cap on Rebar
22	329632.3755	350848.0945	80+46.65	-497.07	Found Aluminum Cap
23	329086.6710	351277.0483	74+99.35	-70.16	Found Rebar
24	329066.5498	351296.9664	74+79.15	-50.32	Found Rebar
25	328738.6101	351298.3296	71+51.18	-50.15	Found Aluminum Cap
26	327307.5531	351386.5762	57+19.83	32.93	Found Rebar
27	328726.6298	351348.3960	71+39.04	-0.12	Found Aluminum Cap
30	327024.2183	352165.9841	54+33.59	811.27	Found Rebar
31	327024.1763	352015.8438	54+34.11	661.13	Found Rebar
33	327023.0931	351619.0875	54+34.51	264.37	Found Aluminum Cap
36	329146.0005	350056.8953	75+63.24	-1,290.08	Found Rebar
39	327023.6948	351883.4812	54+34.12	528.77	Found Aluminum Cap Monument
40	329086.2355	350056.7780	75+03.47	-1,290.42	Found Aluminum Cap Monument
41	326932.2792	351354.7337	53+44.88	-0.32	Found Aluminum Cap Monument
42	326952.3764	351354.8897	53+64.78	-0.09	Found Aluminum Cap Monument
43	326951.5573	350034.3428	53+68.89	-1,320.63	Found Brass Cap Monument
44	329592.4911	351345.2637	80+04.91	-0.06	Found Rebar
45	329632.7085	351265.0689	80+45.43	-80.10	Found Rebar
47	329560.9317	350809.7803	79+75.35	-535.65	Found Rebar
48	329354.6282	350810.0764	77+69.05	-536.13	Found Plastic Cap on Rebar
49	329532.8617	351515.5302	79+44.65	169.99	Found Rebar
50	329262.9962	351516.3143	76+74.78	169.76	Found Aluminum Cap
51	328272.2963	350689.7222	66+86.80	-660.23	Found Aluminum Cap
52	329213.0188	351526.6275	76+24.76	179.89	Found Rebar
54	329594.9544	352508.1194	80+03.03	1,162.80	Found Rebar
55	327948.2770	351656.2906	63+59.52	305.09	Found Rebar
56	327582.8231	351292.4239	59+95.46	-60.17	Found Plastic Cap on Rebar
57	327120.4727	351387.3803	55+32.75	33.03	Found Rebar
58	326962.8626	355320.5669			Found Brass Cap Monument
59	329354.4932	350996.0909	77+68.22	-350.12	Found Plastic Cap on Rebar
60	329427.9923	351515.8887	78+39.78	169.95	Found Aluminum Cap
62	326959.5302	353999.8780			Found Brass Cap Monument
103	328272.5145	351349.9523	66+84.93	0.00	Found Aluminum Cap
104	327587.5915	351352.5727	60+00.00	-0.01	Found Aluminum Cap
107	328273.3293	351679.9656	66+84.48	330.01	Found Rebar
112	327582.4353	351039.3350	59+96.02	-313.26	Found Rebar
113	327611.9218	350692.2811	60+26.86	-660.20	Found Aluminum Cap
114	327581.9176	350692.3456	59+96.82	-660.25	Found Aluminum Cap Monument
115	327642.1808	350888.9135	60+56.36	-463.45	Found Rebar
116	327642.1967	351019.3249	60+55.88	-333.04	Found Rebar
117	327641.9852	350947.5638	60+55.94	-404.80	Found Rebar
118	327802.7474	351301.5526	62+15.35	-50.20	Found Rebar
119	327947.7839	351381.2053	63+60.08	30.01	Found Rebar
121	327563.6871	351881.5248	59+74.10	528.85	Found Plastic Cap on Rebar
122	327563.9541	352029.9253	59+73.80	677.25	Found Plastic Cap on Rebar
123	327551.5210	352062.9374	59+61.25	710.22	Found Plastic Cap on Rebar
124	328340.5625	351307.7252	67+53.11	-42.01	Found Rebar
125	328317.5614	351284.8078	67+30.18	-65.00	Found Rebar
126	328317.5523	351255.2396	67+30.26	-94.57	Found Rebar
127	328324.5062	351255.2319	67+37.22	-94.56	Found Rebar



NOTES

1. THE FIELD SURVEY WAS PERFORMED BY R&M CONSULTANTS, INC. FROM OCTOBER 12, 2011 TO NOVEMBER 19, 2011 AND FROM SEPTEMBER 28, 2012 TO OCTOBER 11, 2012. FIELD SURVEY INFORMATION FOR THIS PROJECT IS LOCATED IN M.O.A. FIELD BOOK NO. 3576, PAGES 1 THROUGH 73 AND M.O.A. FIELD BOOK NO. 3589, PAGES 1 THROUGH 49.
2. A TITLE SEARCH WAS NOT PERFORMED, EASEMENTS OF RECORD OTHER THAN THOSE SHOWN ON THE RECORDED PLATS, OR PROVIDED BY THE MUNICIPAL SURVEYOR, ARE NOT SHOWN HEREON.
3. THE BASIS OF COORDINATES IS NGS STATION O'MALLEY, LOCATED NEAR THE INTERSECTION OF THE NEW SEWARD HIGHWAY AND O'MALLEY ROAD. SAID STATION HAS ANCHORAGE BOWL 2000 COORDINATES OF 303939.2310 N, 353362.5446 E. (U.S. SURVEY FEET).
4. THE BASIS OF BEARINGS IS A LOCAL PLANE BEARING BETWEEN NGS STATION O'MALLEY AND NGS STATION LOOP 2 USE RM 3 1964. NGS STATION LOOP 2 USE RM 3 1964 BEARS N 01°43'26.4" E A DISTANCE OF 49488.4476 FEET FROM NGS STATION O'MALLEY. NGS STATION LOOP 2 USE RM 3 1964 HAS ANCHORAGE BOWL 2000 COORDINATES OF 353405.2778 N, 354851.3982 E. (U.S. SURVEY FEET)
5. ELEVATIONS ARE BASED ON THE M.O.A. VERTICAL DATUM, 1972 N.G.S. ADJUSTMENT, FROM M.O.A. BENCHMARK "GAAB 93", ELEV. = 113.95'. SEE MOA BENCHMARK NETWORK BOOK, DESCRIPTION PAGE NO. D-54.
6. ALL DISTURBED PROPERTY CORNERS SHALL BE REPLACED BY THE CONTRACTOR IN ACCORDANCE WITH THE M.O.A. STANDARD SPECIFICATIONS, SECTION 65.02 CONSTRUCTION SURVEYING, ARTICLE 2.1 PROJECT CONTROL.

LEGEND

- ⊕ FOUND ALUMINUM OR BRASS CAP MONUMENT
- ⊙ FOUND REBAR WITH ALUMINUM OR PLASTIC CAP
- FOUND REBAR
- ⑤ SURVEY CONTROL POINT NUMBER, SEE COORDINATE SCHEDULE

FIELD BOOKS	BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REV	DATE	DESCRIPTION	BY
DESIGN: MOA Book 3576, Pages 1 - 73	GAAB 93	MOA 1988 Benchmark Book, D-54	113.95'	BASE	R&M		TELEPHONE						
MOA Book 3589, Pages 1 - 55				TOPOGRAPHY			ELECTRIC						
STAKING:				PROFILE			CABLE TV						
ASBUILT:				SANITARY SEWER			TRAFFIC SIGNAL						
CONTRACTOR:				STORM SEWER			DESIGN						
INSPECTOR:				WATER			QUANTITIES						
CONSTRUCTION RECORD				GAS			MUN. FINAL CHECK						

R&M CONSULTANTS, INC.
 8101 Vanguard Drive
 Anchorage, Alaska 99507
 907 552 1707 voice
 907 552 3434 fax
 www.rmaconsult.com

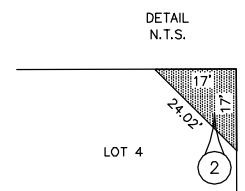
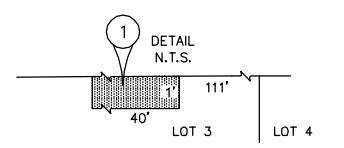
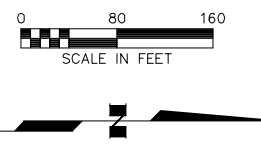
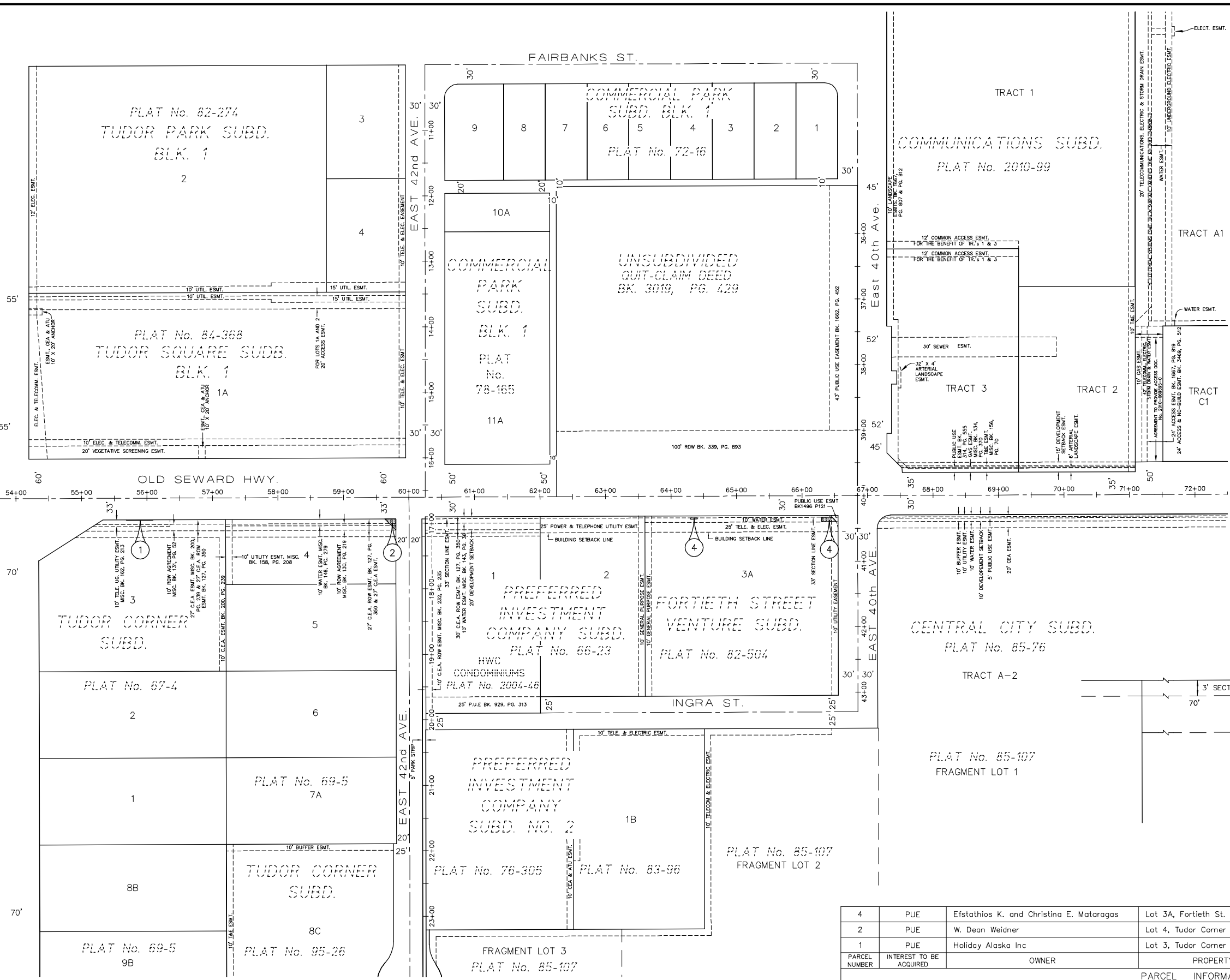
STATE OF ALASKA
 9th
 WILLIAM D. COHEN
 No. LS-7537
 REGISTERED PROFESSIONAL LAND SURVEYOR
 1/1/2015



PUBLIC WORKS DEPARTMENT
 10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH. A-D
CONTROL SURVEY
 34TH AVENUE TO TUDOR ROAD
 HORZ SCALE: 1"=80' DATE: JAN. 2013 GRID SW731, SW732
 VERT SCALE: N/A ACCT. NO. SHEET G5 of G9

E. TUDOR RD.

E. TUDOR RD.

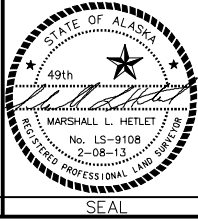


DETAIL N.T.S.
 $\Delta = 28^\circ 12' 42''$
 $R = 20.00'$
 $L = 9.85'$
 $CB = S 45^\circ 40' 30'' W$
 $CD = 9.75'$

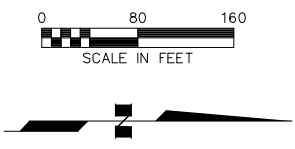
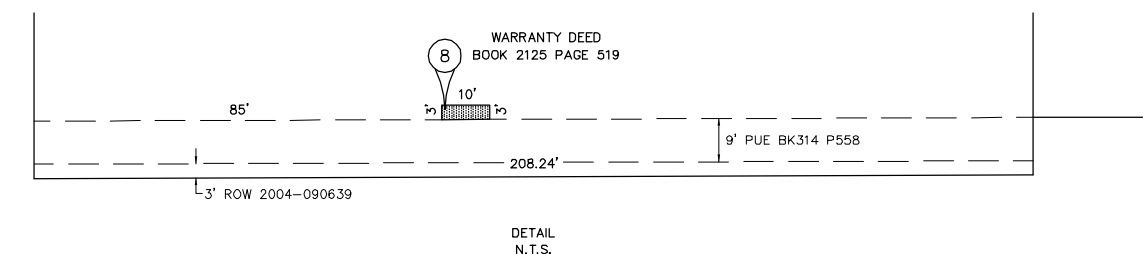
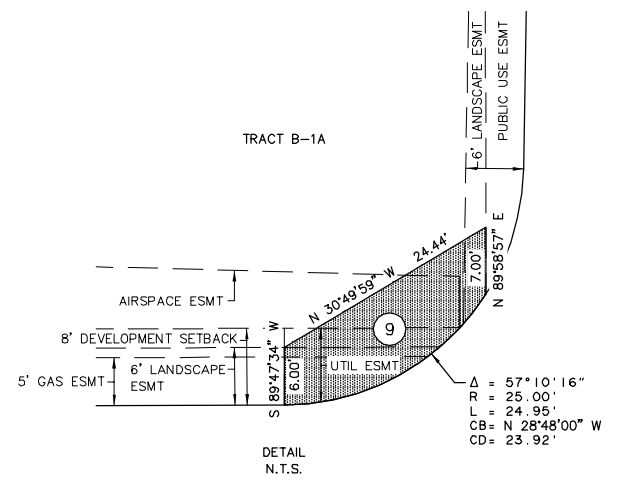
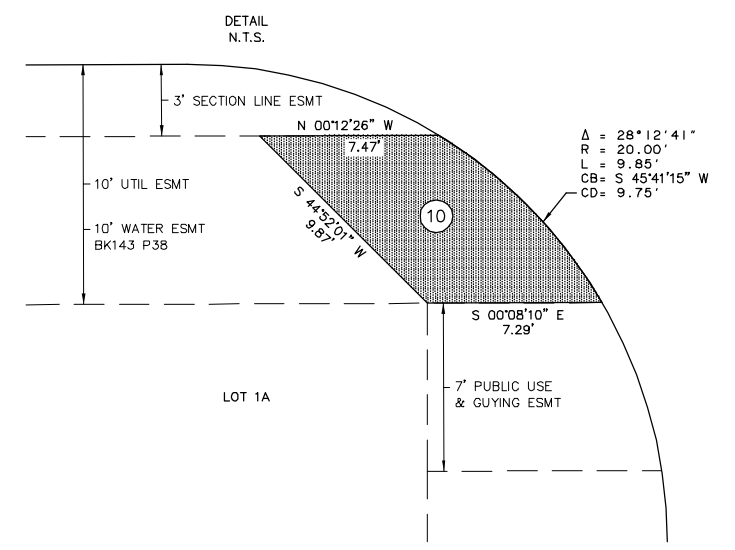
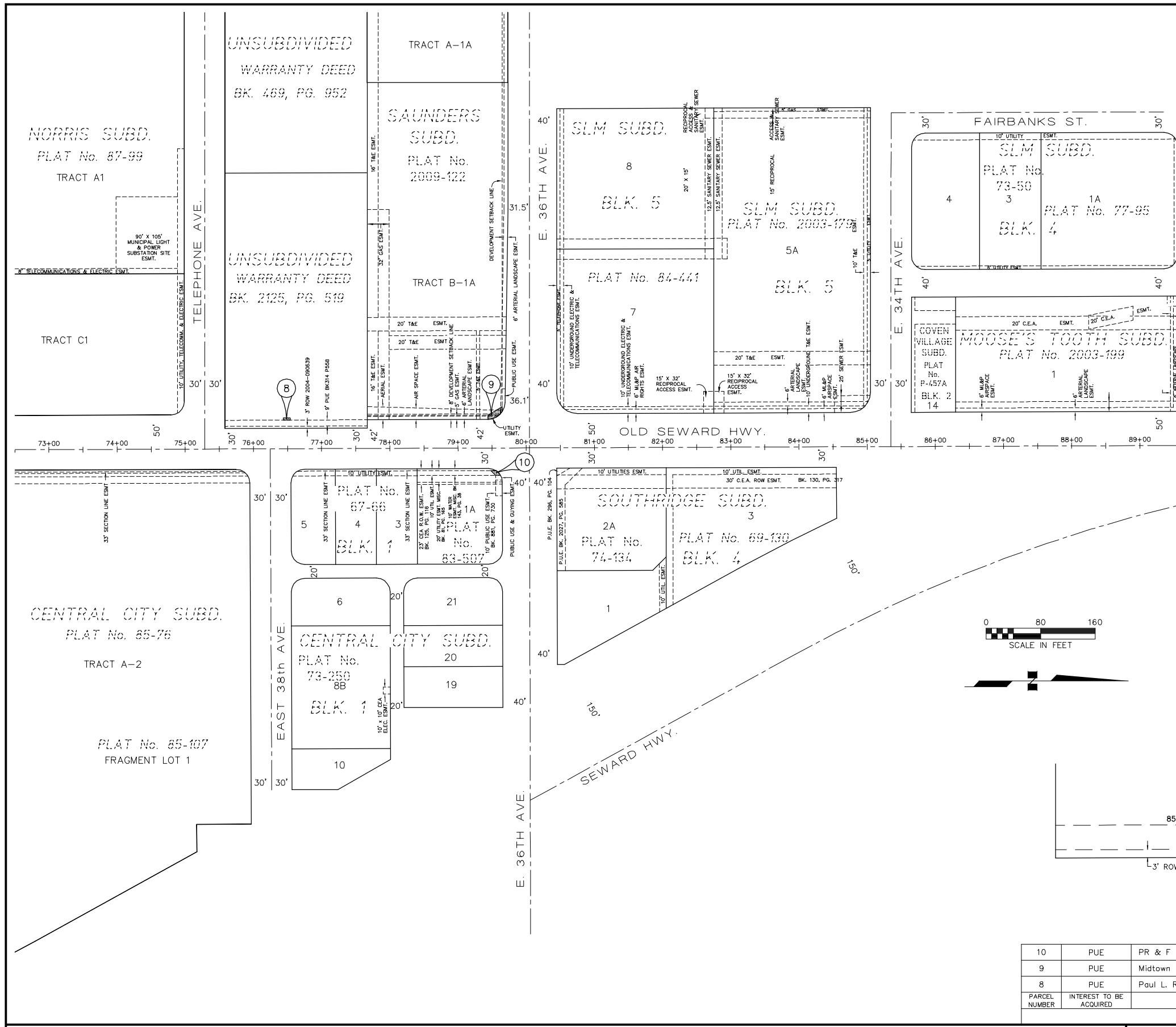
PARCEL NUMBER	INTEREST TO BE ACQUIRED	OWNER	PROPERTY DESCRIPTION	LARGER PARCEL	NET TAKE	RECORDED DOCUMENT NUMBER
4	PUE	Efstathios K. and Christina E. Mataragas	Lot 3A, Fortieth St. Venture Subd. (82-504)	81,096 S.F.	157 S.F.	2013-XXXXXX
2	PUE	W. Dean Weidner	Lot 4, Tudor Corner Subd. (67-4)	25,815 S.F.	145 S.F.	2013-XXXXXX
1	PUE	Holiday Alaska Inc	Lot 3, Tudor Corner Subd. (67-4)	62,966 S.F.	40 S.F.	2013-XXXXXX

FIELD BOOKS	BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REV	DATE	DESCRIPTION	BY
				BASE			TELEPHONE						
				TOPOGRAPHY			ELECTRIC						
				PROFILE			CABLE TV						
				SANITARY SEWER			TRAFFIC SIGNAL						
				STORM SEWER			DESIGN						
				WATER			QUANTITIES						
				GAS			MUN. FINAL CHECK						

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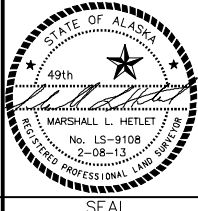
PUBLIC WORKS DEPARTMENT
 10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH. A-D
RIGHT OF WAY MAP
 34TH AVENUE TO TUDOR ROAD
 HORZ SCALE: 1"=80'
 VERT SCALE: N/A
 DATE: FEB 2013
 GRID: SW1731, SW1732
 SHEET: G6 of G9



PARCEL NUMBER	INTEREST TO BE ACQUIRED	OWNER	PROPERTY DESCRIPTION	LARGER PARCEL	NET TAKE	RECORDED DOCUMENT NUMBER
10	PUE	PR & F Joint Venture, a Alaska Partnership	Lot 1, Blk 1, Central City Subd. (83-507)	17,316 S.F.	56 S.F.	2013-XXXXXX
9	PUE	Midtown Market-Sa Properties LLC	Tract B-1A, Saunders Subd. (2009-122)	101,044 S.F.	186 S.F.	2013-XXXXXX
8	PUE	Paul L. Reid	See Warranty Deed Book 2125 Page 519	78,082 S.F.	30 S.F.	2013-XXXXXX

FIELD BOOKS	BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REV	DATE	DESCRIPTION	BY
DESIGN:				BASE			TELEPHONE						
STAKING:				TOPOGRAPHY			ELECTRIC						
ASBUILT:				PROFILE			CABLE TV						
CONTRACTOR:				SANITARY SEWER			TRAFFIC SIGNAL						
INSPECTOR:				DESIGN			DESIGN						
CONSTRUCTION RECORD				WATER			QUANTITIES						
				GAS			MUN. FINAL CHECK						

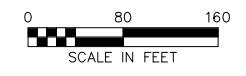
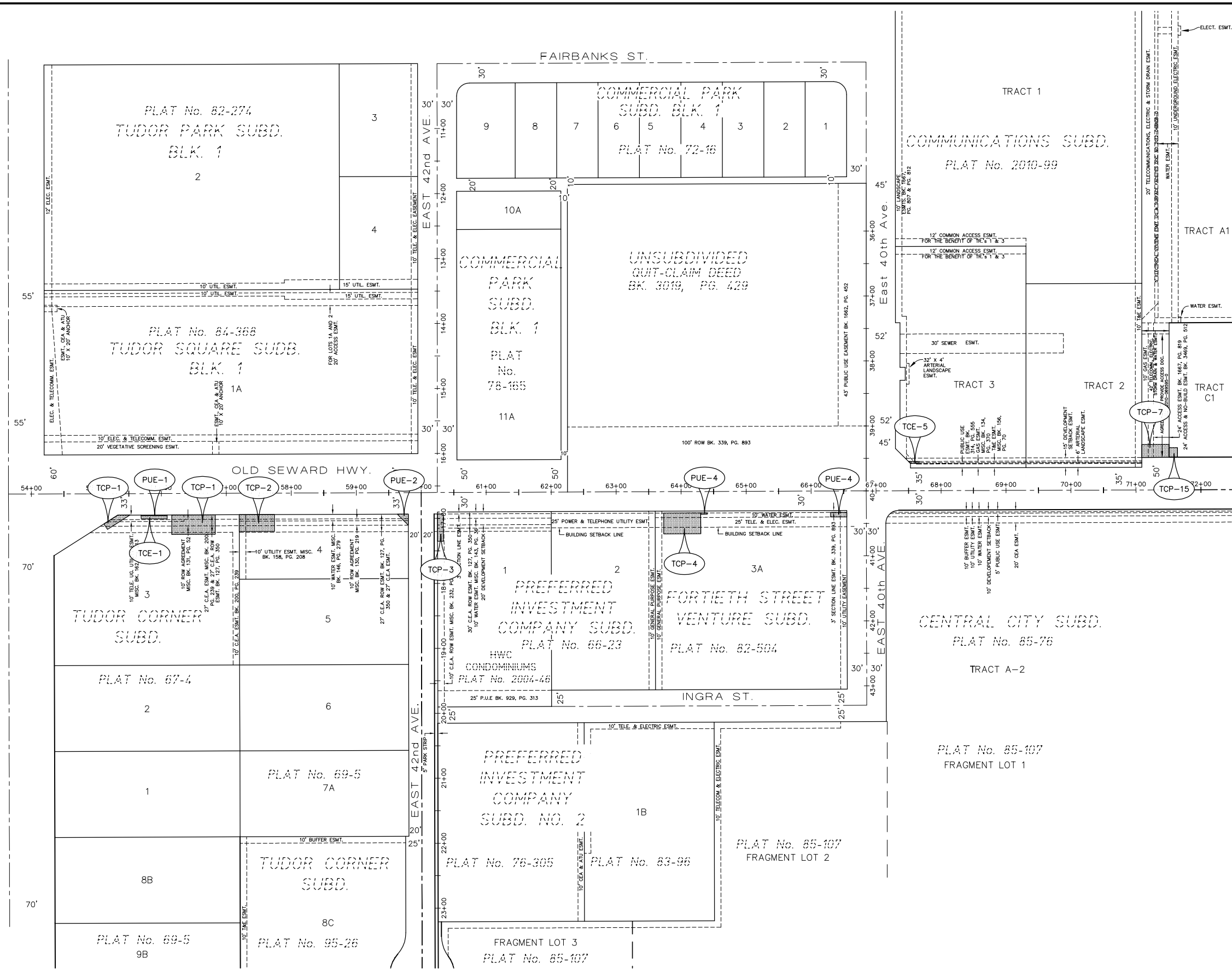
USKH
2515 'A' STREET
ANCHORAGE, AK 99503
(907) 276-4245
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PUBLIC WORKS DEPARTMENT
10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH. A-D
RIGHT OF WAY MAP
34TH AVENUE TO TUDOR ROAD
HORIZ SCALE: 1"=80' DATE: FEB 2013 GRID: SW1731, SW1732 SHEET: G7 of G9
VERT SCALE: N/A ACCT. NO.

E. TUDOR RD.

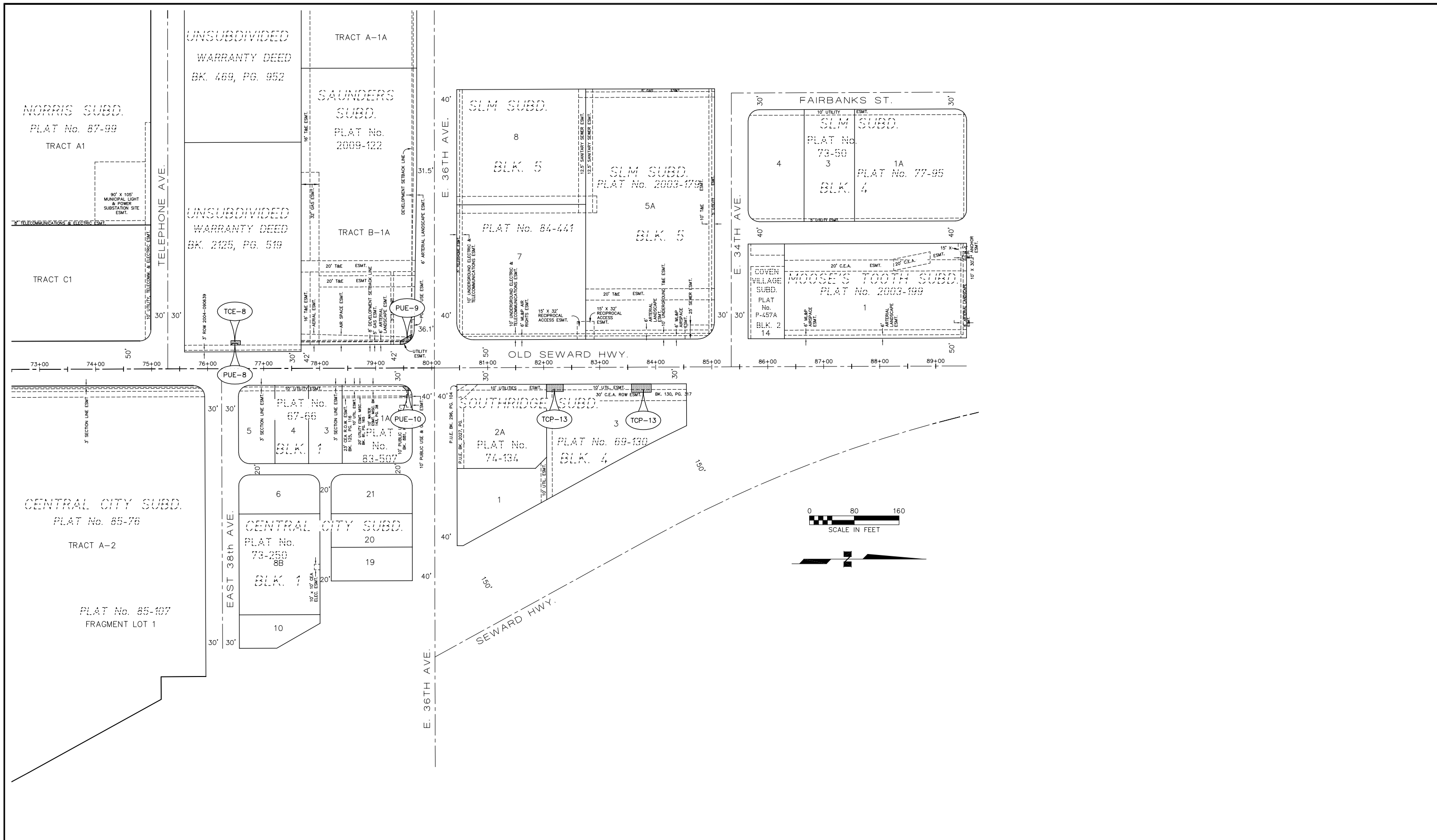
E. TUDOR RD.



FIELD BOOKS	BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REV	DATE	BY	REV	DATE	DESCRIPTION	BY
DESIGN:				BASE			TELEPHONE									
STAKING:				TOPOGRAPHY			ELECTRIC									
ASBUILT:				PROFILE			CABLE TV									
CONTRACTOR:				SANITARY SEWER			TRAFFIC SIGNAL									
INSPECTOR:				STORM SEWER			DESIGN									
CONSTRUCTION RECORD				WATER			QUANTITIES									
				GAS			MUN. FINAL CHECK									
							PLAN CHECK									

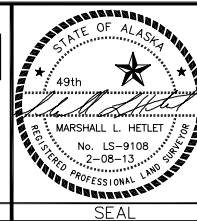
USKH
 2515 'A' STREET
 ANCHORAGE, AK 99503
 (907) 276-4245
 USKH.COM

PUBLIC WORKS DEPARTMENT
 10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH. A-D
EASEMENT & PERMIT INDEX MAP
 34TH AVENUE TO TUDOR ROAD
 HORZ SCALE: 1"=80' DATE: JAN 2013 GRID: SW1731, SW1732
 VERT SCALE: N/A ACCT. NO. SHEET 68 of 69



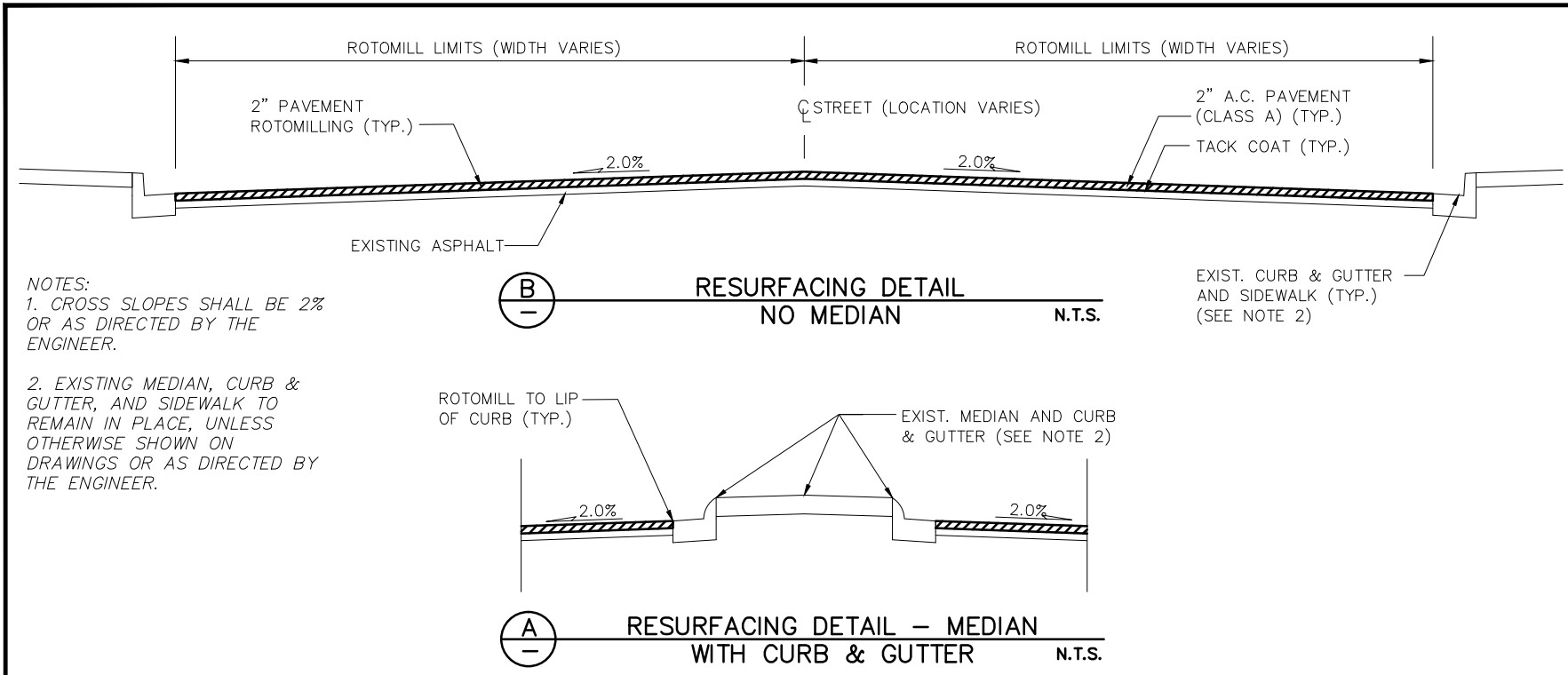
FIELD BOOKS	BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REV	DATE	BY	REV	DATE	DESCRIPTION	BY
DESIGN:				BASE			TELEPHONE									
STAKING:				TOPOGRAPHY			ELECTRIC									
ASBUILT:				PROFILE			CABLE TV									
CONTRACTOR:				SANITARY SEWER			TRAFFIC SIGNAL									
INSPECTOR:				STORM SEWER			DESIGN									
				WATER			QUANTITIES									
CONSTRUCTION RECORD				GAS			MUN. FINAL CHECK									
		VERTICAL DATUM					PLAN CHECK									

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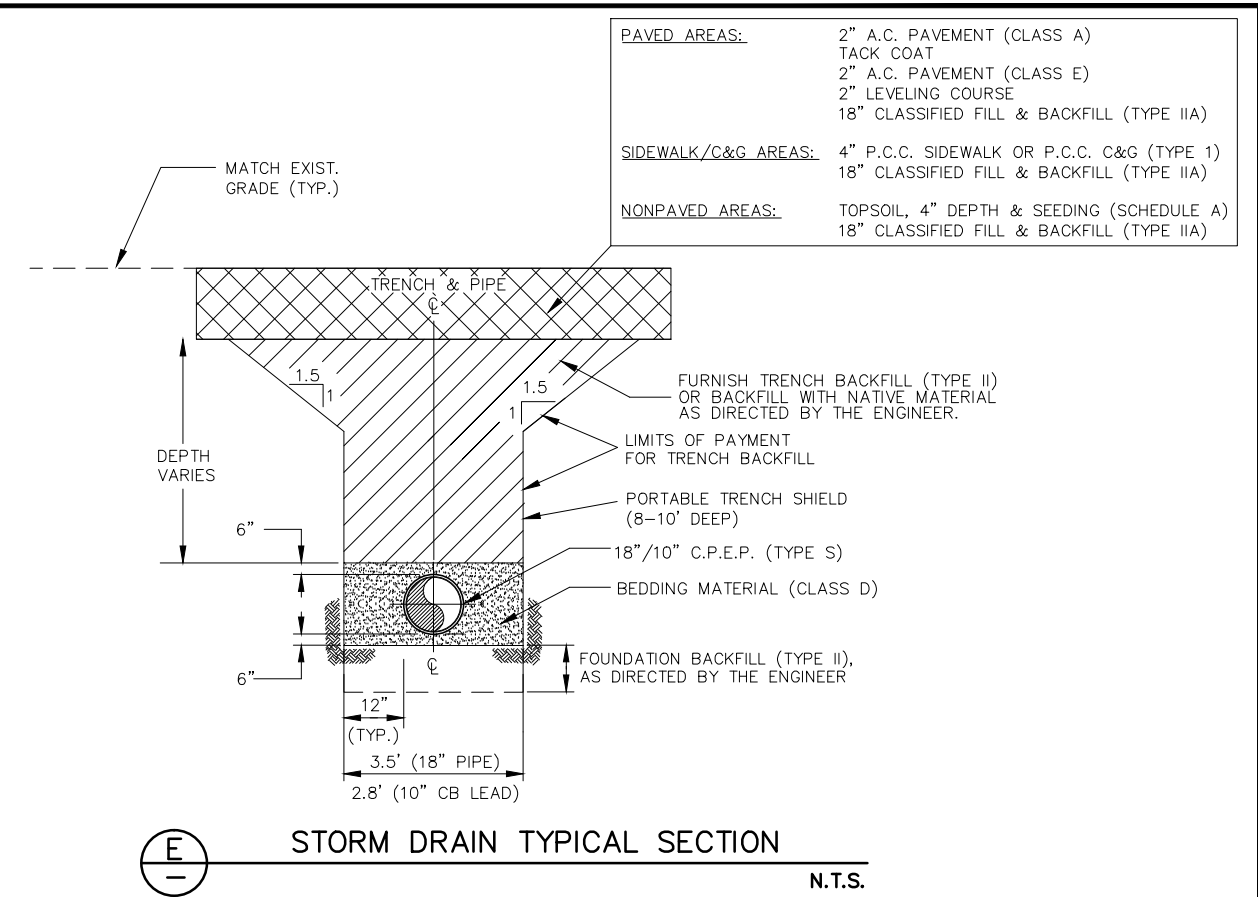


PUBLIC WORKS DEPARTMENT			
10-13	OLD SEWARD HIGHWAY SURFACE REHABILITATION	SCH. A-D	
EASEMENT & PERMIT INDEX MAP			
34TH AVENUE TO TUDOR ROAD			
HORIZ SCALE: 1"=80'	DATE: JAN 2013	GRID: SW1731, SW1732	SHEET: G9 of G9
VERT SCALE: N/A	ACCT. NO.		

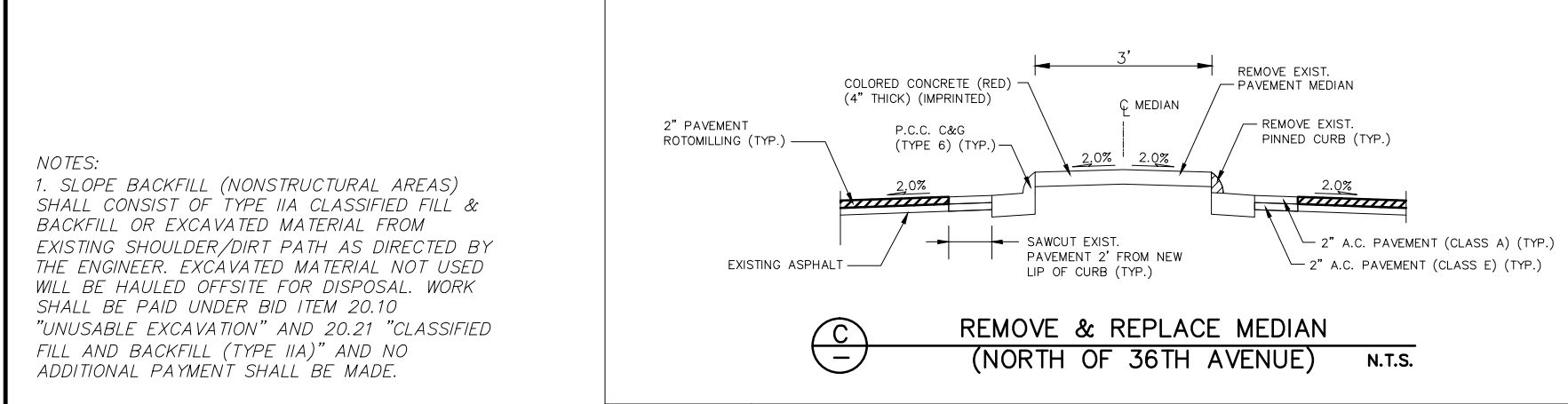
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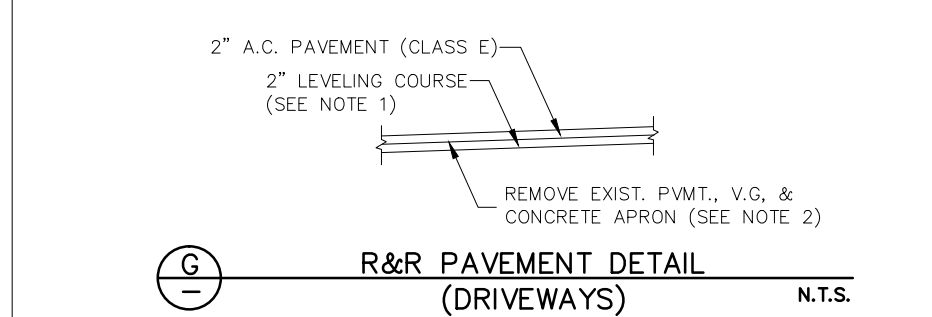
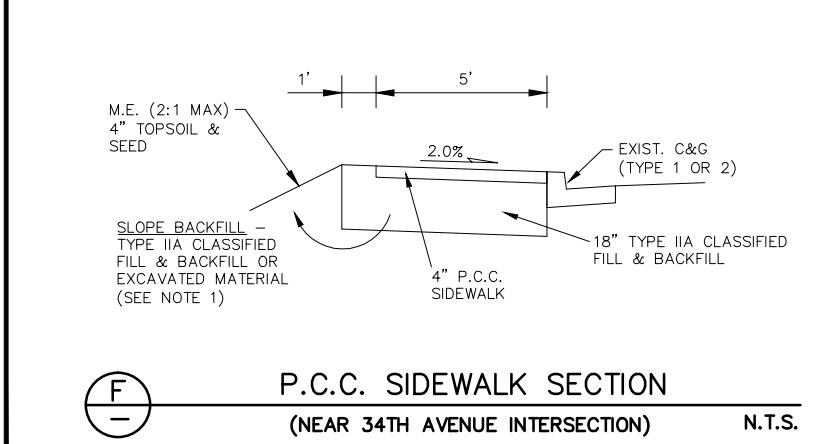
NOTES:
 1. CROSS SLOPES SHALL BE 2% OR AS DIRECTED BY THE ENGINEER.
 2. EXISTING MEDIAN, CURB & GUTTER, AND SIDEWALK TO REMAIN IN PLACE, UNLESS OTHERWISE SHOWN ON DRAWINGS OR AS DIRECTED BY THE ENGINEER.



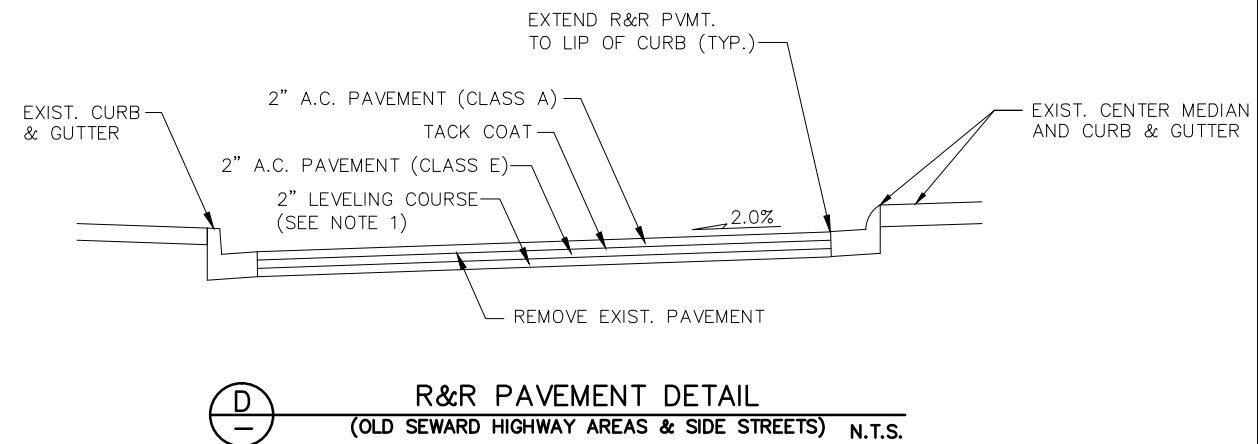
NOTES:
 1. SLOPE TRENCH WALLS ACCORDING TO SOIL CONDITIONS AND O.S.H.A. SAFETY STANDARDS.
 2. BENEATH PAVED AREAS, PLACE AND COMPACT TO NOT LESS THAN 95% OF THE MAXIMUM DENSITY AT OPTIMUM MOISTURE CONTENT. IN NON-STRUCTURAL AREAS, COMPACT TO NOT LESS THAN 85% OF THE MAXIMUM DENSITY AT OPTIMUM MOISTURE CONTENT.



NOTES:
 1. SLOPE BACKFILL (NONSTRUCTURAL AREAS) SHALL CONSIST OF TYPE IIA CLASSIFIED FILL & BACKFILL OR EXCAVATED MATERIAL FROM EXISTING SHOULDER/DIRT PATH AS DIRECTED BY THE ENGINEER. EXCAVATED MATERIAL NOT USED WILL BE HAULED OFFSITE FOR DISPOSAL. WORK SHALL BE PAID UNDER BID ITEM 20.10 "UNUSABLE EXCAVATION" AND 20.21 "CLASSIFIED FILL AND BACKFILL (TYPE IIA)" AND NO ADDITIONAL PAYMENT SHALL BE MADE.

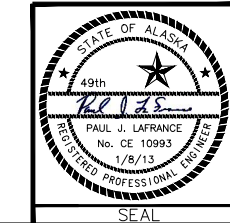


NOTES:
 1. THICKNESS OF LEVELING COURSE WILL BE AS NEEDED AND AS DIRECTED BY THE ENGINEER.
 2. REMOVAL OF VALLEY GUTTER (V.G.) & CONCRETE APRONS IN DRIVEWAY AREAS WILL BE PAID FOR AS REMOVE PAVEMENT & NO ADDITIONAL PAYMENT SHALL BE MADE.



NOTES:
 1. THICKNESS OF LEVELING COURSE WILL BE AS NEEDED AND AS DIRECTED BY THE ENGINEER. PROVIDE 4" OF LEVELING COURSE IN AREAS WHERE DETECTOR LOOPS ARE INSTALLED.

FIELD BOOKS	DATA	DRAWN BY	CHECKED BY	REV	DATE	DESCRIPTION	BY
DESIGN: N/A	DESIGN						
STAKING:	QUANTITIES						
ASBUILT:	MUN. FINAL CHECK						
CONTRACTOR:							
INSPECTOR:							
CONSTRUCTION RECORD	PLAN CHECK					REVISIONS	



PUBLIC WORKS DEPARTMENT

10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH. A-B

TYPICAL SECTIONS & DETAILS

34TH AVENUE TO TUDOR ROAD

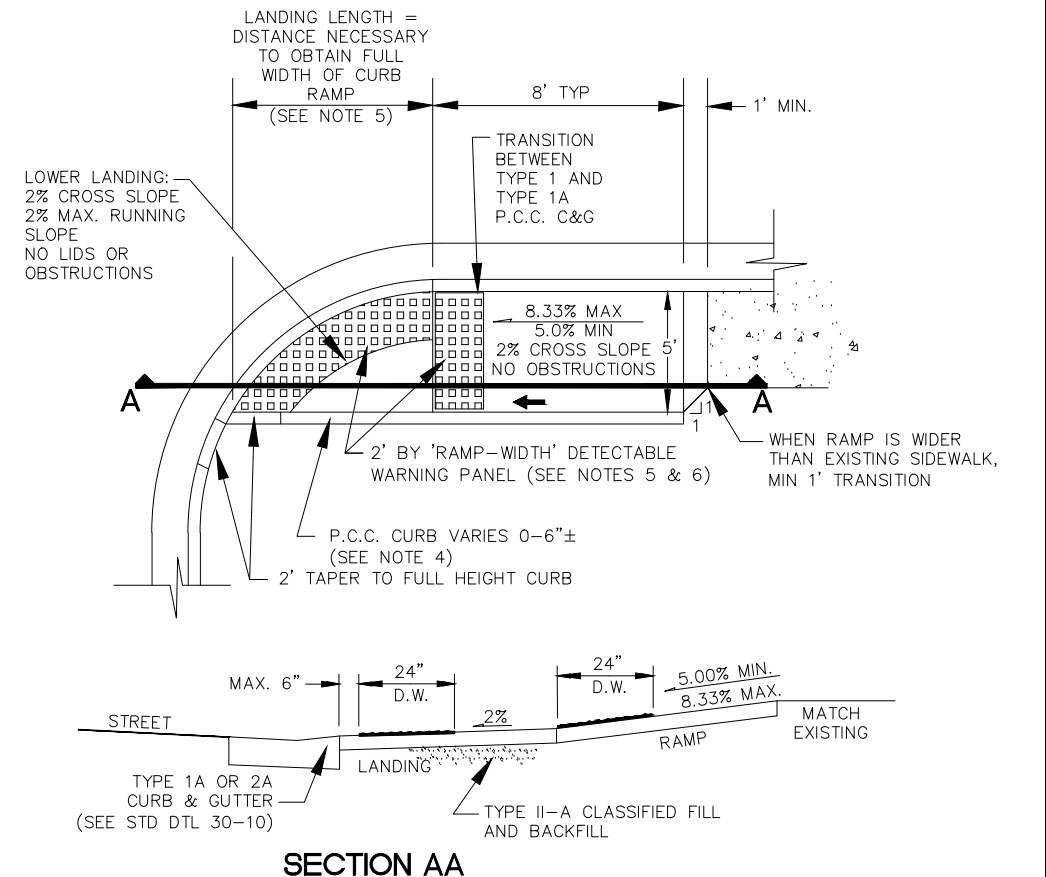
SCALE: N.T.S. DATE: JAN 2013 GRID: SW1631, SW1632, SW1731, SW1732 SHEET D1 of D3

G:\Public Works\PM\10-13 Old Seward Hwy Surface Rehab - 34th Ave to Tudor Rd\Design\ENG\dwg\10-13 Typ & ADA.dwg

ADA RAMP SUMMARY

ID	STA	OFFSET	TYPE	DETECTABLE WARNING	R&R C&G QTY. (L.F.)	REMARKS
A	59+88±	51± LT	UNIDIRECTIONAL	2' x 15' @ 30' RADIUS	24'	R&R UNIDIRECTIONAL RAMP IAW DETAIL A. COORDINATE WITH ELECTRICAL.
B	60+51±	41± LT	UNIDIRECTIONAL	2' x 12' @ 20' RADIUS	26'	R&R UNIDIRECTIONAL RAMP IAW DETAIL A. COORDINATE WITH ELECTRICAL.
C	59+76±	31± RT	PARALLEL	2' x 5'	35'	R&R PARALLEL RAMP IAW STD DTL 30-09.
D	60+23±	29± RT	PARALLEL	2' x 5'	26'	R&R PARALLEL RAMP IAW STD DTL 30-09.
E	66+43±	49± LT	PARALLEL	2' x 5'	22'	R&R PARALLEL RAMP IAW STD DTL 30-09. COORDINATE WITH ELECTRICAL.
F	67+28±	48± LT	PARALLEL	2' x 5'	19'	R&R PARALLEL RAMP IAW STD DTL 30-09. COORDINATE WITH ELECTRICAL.
G	66+48±	27± RT	PARALLEL	2' x 5'	24'	R&R PARALLEL RAMP IAW STD DTL 30-09.
H	66+61±	38± RT	PARALLEL	2' x 5'	24'	R&R PARALLEL RAMP IAW STD DTL 30-09. EXTEND CONCRETE LANDING/SIDEWALK TO SIGNAL POLE AS DIRECTED BY THE ENGINEER. LANDING SLOPE NO GREATER THAN 2%. REGRADE ADJACENT SLOPES TO PROVIDE ACCESS TO SIGNAL POLE FOUNDATION BOLTS. RESEED DISTURBED AREAS. WORK SHALL BE PAID UNDER 30.03 "P.C.C. SIDEWALK (6" THICK)" AND NO ADDITIONAL PAYMENT SHALL BE MADE.
I	71+10±	40± LT	UNIDIRECTIONAL	2' x 12' @ 20' RADIUS	34'	R&R UNIDIRECTIONAL RAMP IAW DETAIL A.
J	71+66±	39± LT	UNIDIRECTIONAL	2' x 12' @ 20' RADIUS	34'	R&R UNIDIRECTIONAL RAMP IAW DETAIL A. COORDINATE WITH ELECTRICAL.
K	75+58±	38± LT	UNIDIRECTIONAL	2' x 12' @ 20' RADIUS	34'	R&R UNIDIRECTIONAL RAMP IAW DETAIL A.
L	79+68±	49± LT	PERPENDICULAR	2' x 5'	18'	R&R PERPENDICULAR RAMP IAW STD DTL 30-08 AND AS DIRECTED BY THE ENGINEER. COORDINATE WITH ELECTRICAL.
M	76+51±	29± RT	PARALLEL	2' x 5'	24'	R&R PARALLEL RAMP IAW STD DTL 30-09.
N	79+55±	25± RT	PERPENDICULAR	2' x 5'	18'	R&R PERPENDICULAR RAMP IAW STD DTL 30-08 AND AS DIRECTED BY THE ENGINEER. COORDINATE WITH ELECTRICAL.
O	80+56±	46± LT	PARALLEL	2' x 5'	25'	R&R PARALLEL RAMP IAW STD DTL 30-09. COORDINATE WITH ELECTRICAL.
P	84+93±	42± LT	UNIDIRECTIONAL	2' x 15' @ 40' RADIUS	26'	CONSTRUCT UNIDIRECTIONAL RAMP IAW DETAIL A. SEE NOTE 3.
Q	85+57±	42± LT	PERPENDICULAR	2' x 5'	18'	CONSTRUCT PERPENDICULAR RAMP IAW STD. DTLS. 30-08 & 30-11. SEE NOTE 3.
R	40+27±	37± LT	PARALLEL	2' x 5'	25'	IF C&G IS ATTACHED TO ADA RAMP (MONOLITHIC POUR), R&R PARALLEL RAMP & RAMP LANDINGS AS DIRECTED BY THE ENGINEER AND I.A.W. STD DTL 30-09. ADJUST JUNCTION BOX TO GRADE. IF C&G AND ADA RAMP ARE SEPARATE, R&R C&G ONLY.

- NOTES:
- CONTRACTOR SHALL CONSTRUCT ALL RAMP RUN SLOPES EQUAL TO OR LESS THAN THE 8.33% MAXIMUM. WHERE NECESSARY, CONTRACTOR SHALL SAW-CUT THE LANDING AREA BEHIND PERPENDICULAR RAMP IN ORDER TO ACHIEVE SUFFICIENT DISTANCE TO OBTAIN THE CORRECT SLOPE.
 - ADDITIONAL SIDEWALK/CURB RAMP CONSTRUCTION INFORMATION IS CONTAINED IN THE GENERAL NOTES.
 - UNDER NEW RAMPS P & Q EXCAVATE 18" AND PLACE 18" CLASSIFIED FILL & BACKFILL (TYPE IIA) UNDER RAMPS, OR AS DIRECTED BY THE ENGINEER. WORK SHALL BE PAID UNDER 20.10 "UNUSABLE EXCAVATION" AND 20.21 "CLASSIFIED FILL AND BACKFILL (TYPE II-A)" AND NO ADDITIONAL PAYMENT SHALL BE MADE.



- RAMP NOTES:**
- CONSTRUCT UNIDIRECTIONAL RAMPS AND LANDINGS WITH A BROOM FINISH PERPENDICULAR TO THE LONG DIRECTION OF THE RAMP.
 - CONTRACTOR SHALL CONSTRUCT THE RAMP PORTION OF THE CURB RAMP WITH A 2% CROSS SLOPE WITH NO MANHOLES, UTILITY JUNCTION BOXES, OR OTHER OBSTRUCTIONS. THE RUNNING SLOPE IS 5% MINIMUM AND 8.33% MAXIMUM, BUT SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET.
 - CONTRACTOR SHALL CONSTRUCT LANDINGS WITH A MAXIMUM 2% RUNNING SLOPE AND 2% CROSS SLOPE.
 - CONTRACTOR SHALL CONSTRUCT P.C.C. CURB BEHIND LANDING AND RAMPS WHERE SHOWN OR AS DIRECTED BY THE ENGINEER. P.C.C. CURB IS INCIDENTAL TO CURB RAMP AND NO ADDITIONAL PAYMENT WILL BE MADE.
 - IF LANDING LENGTH IS LESS THAN 5', CONTRACTOR SHALL INSTALL DETECTABLE WARNINGS AT THE BOTTOM OF THE RAMP. IF THE LANDING LENGTH IS EQUAL TO OR GREATER THAN 5', CONTRACTOR SHALL INSTALL RADIAL DETECTABLE WARNING ALONG TOP BACK OF CURB FOR THE WIDTH OF THE LANDING. DETECTABLE WARNINGS SHALL BE INSTALLED IAW MANUFACTURERS RECOMMENDATIONS AND ALIGNED SUCH THAT THE TRUNCATED DOMES ARE IN LINE WITH THE DIRECTION OF TRAVEL.
 - CONTRACTOR SHALL INSTALL 24 INCH DETECTABLE WARNINGS I.A.W. MANUFACTURERS' RECOMMENDATIONS AND THESE DRAWINGS. D.W. SHALL EXTEND THE FULL WIDTH OF THE LANDING. INSTALL D.W. SO THAT THE FIELD AREA AT THE BASE OF THE DOMES IS FLUSH WITH THE SURROUNDING CONCRETE. ALLOW NO LIPS AT EDGE OF THE D.W. OR FLOW LINE. D.W. SHALL BE FEDERAL YELLOW, OR APPROVED EQUAL. WHERE MANUFACTURED RADIAL D.W. ARE NOT AVAILABLE, CONTRACTOR SHALL CUT D.W. I.A.W. MANUFACTURER'S RECOMMENDATIONS TO PROVIDE RADIAL D.W. AS SHOWN ON THE DRAWINGS.

(A) UNIDIRECTIONAL CURB RAMP
DETAIL N.T.S.

FIELD BOOKS		DATA	DRAWN BY	CHECKED BY	REV	DATE	DESCRIPTION	BY
DESIGN: N/A		DESIGN						
STAKING:		QUANTITIES						
ASBUILT:		MUN. FINAL CHECK						
CONTRACTOR:								
INSPECTOR:								
CONSTRUCTION RECORD		PLAN CHECK					REVISIONS	

PUBLIC WORKS DEPARTMENT

10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH. A&D

ADA WORK & UNIDIRECTIONAL CURB RAMP DETAIL

34TH AVENUE TO TUDOR ROAD

SCALE: N.T.S. DATE: JAN 2013 GRID: SW1631, SW1632, SW1731, SW1732 SHEET D2 of D3


ACCT. NO.

G:\Public Works\PM\10-13 Old Seward Hwy Surface Rehab - 34th Ave to Tudor Rd\Design\ENG\dwg\10-13 Typ & ADA.dwg

ADA RAMP SUMMARY – DRIVEWAYS (NO DETECTABLE WARNINGS)					
ID	STA	OFFSET	TYPE	R&R C&G QTY. (L.F.)	REMARKS
DW1	55+24±	31± RT	PARALLEL	25'	R&R DRIVEWAY PARALLEL RAMP AS DIRECTED BY THE ENGINEER AND IAW STD. DTL. 30-09 EXCEPT FOR NO DETECTABLE WARNING.
DW2	56+22±	28± RT	PARALLEL	25'	R&R DRIVEWAY PARALLEL RAMP AS DIRECTED BY THE ENGINEER AND IAW STD. DTL. 30-09 EXCEPT FOR NO DETECTABLE WARNING.
DW3	56+78±	28± RT	PARALLEL	42'	R&R DRIVEWAY PARALLEL RAMP AS DIRECTED BY THE ENGINEER AND IAW STD. DTL. 30-09 EXCEPT FOR NO DETECTABLE WARNING.
DW4	57+23±	28± RT	PARALLEL	25'	R&R DRIVEWAY PARALLEL RAMP AS DIRECTED BY THE ENGINEER AND IAW STD. DTL. 30-09 EXCEPT FOR NO DETECTABLE WARNING.
DW5	57+67±	28± RT	PARALLEL	20'	R&R DRIVEWAY PARALLEL RAMP AS DIRECTED BY THE ENGINEER AND IAW STD. DTL. 30-09 EXCEPT FOR NO DETECTABLE WARNING.
DW6	64+10±	40± LT	UNIDIRECTIONAL	55'	R&R DRIVEWAY UNIDIRECTIONAL RAMP AS DIRECTED BY THE ENGINEER AND IAW DETAIL A EXCEPT FOR NO DETECTABLE WARNING. ADJUST J-BOX TO GRADE.
DW7	64+80±	40± LT	UNIDIRECTIONAL	50'	R&R DRIVEWAY UNIDIRECTIONAL RAMP AS DIRECTED BY THE ENGINEER AND IAW DETAIL A EXCEPT FOR NO DETECTABLE WARNING.
DW8	63+78±	28± RT	UNIDIRECTIONAL	45'	R&R DRIVEWAY UNIDIRECTIONAL RAMP AS DIRECTED BY THE ENGINEER AND IAW DETAIL A EXCEPT FOR NO DETECTABLE WARNING.
DW9	64+26±	28± RT	UNIDIRECTIONAL	48'	R&R DRIVEWAY UNIDIRECTIONAL RAMP AS DIRECTED BY THE ENGINEER AND IAW DETAIL A EXCEPT FOR NO DETECTABLE WARNING.
DW10	85+56±	148± LT	UNIDIRECTIONAL	10'	CONSTRUCT DRIVEWAY UNIDIRECTIONAL RAMP AS DIRECTED BY THE ENGINEER AND IAW DETAIL A EXCEPT FOR NO DETECTABLE WARNING. SEE NOTE 3.
DW11	82+07±	26± RT	UNIDIRECTIONAL	15'	CONSTRUCT DRIVEWAY UNIDIRECTIONAL RAMP AS DIRECTED BY THE ENGINEER AND IAW DETAIL A EXCEPT FOR NO DETECTABLE WARNING. COORDINATE WITH ELECTRICAL.
DW12	82+34±	26± RT	UNIDIRECTIONAL	15'	CONSTRUCT DRIVEWAY UNIDIRECTIONAL RAMP AS DIRECTED BY THE ENGINEER AND IAW DETAIL A EXCEPT FOR NO DETECTABLE WARNING.
DW13	83+60±	26± RT	UNIDIRECTIONAL	15'	CONSTRUCT DRIVEWAY UNIDIRECTIONAL RAMP AS DIRECTED BY THE ENGINEER AND IAW DETAIL A EXCEPT FOR NO DETECTABLE WARNING. COORDINATE WITH ELECTRICAL.
DW14	83+88±	26± RT	UNIDIRECTIONAL	15'	CONSTRUCT DRIVEWAY UNIDIRECTIONAL RAMP AS DIRECTED BY THE ENGINEER AND IAW DETAIL A EXCEPT FOR NO DETECTABLE WARNING.

- NOTES:
- CONTRACTOR SHALL CONSTRUCT ALL RAMP RUN SLOPES EQUAL TO OR LESS THAN THE 8.33% MAXIMUM. WHERE NECESSARY, CONTRACTOR SHALL SAW-CUT THE LANDING AREA BEHIND PERPENDICULAR RAMPS IN ORDER TO ACHIEVE SUFFICIENT DISTANCE TO OBTAIN THE CORRECT SLOPE.
 - ADDITIONAL SIDEWALK/CURB RAMP CONSTRUCTION INFORMATION IS CONTAINED IN THE GENERAL NOTES.
 - UNDER NEW RAMP DW10 EXCAVATE 18" AND PLACE 18" CLASSIFIED FILL & BACKFILL (TYPE IIA) UNDER RAMP, OR AS DIRECTED BY THE ENGINEER. WORK SHALL BE PAID UNDER 20.10 "UNUSABLE EXCAVATION" AND 20.21 "CLASSIFIED FILL AND BACKFILL (TYPE II-A)" AND NO ADDITIONAL PAYMENT SHALL BE MADE.

FIELD BOOKS	DATA	DRAWN BY	CHECKED BY	REV	DATE	DESCRIPTION	BY
DESIGN: N/A	DESIGN						
STAKING:	QUANTITIES						
ASBUILT:	MUN. FINAL CHECK						
CONTRACTOR:							
INSPECTOR:							
CONSTRUCTION RECORD	PLAN CHECK	REVISIONS					

	PUBLIC WORKS DEPARTMENT	
	10-13	SCH. A&D
	OLD SEWARD HIGHWAY SURFACE REHABILITATION	
	DRIVEWAY ADA RAMPS	
34TH AVENUE TO TUDOR ROAD		
SCALE: N.T.S.	DATE: JAN 2013	GRID: SW1631, SW1632 SW1731, SW1732
ACCT. NO.	SHEET D3 of D3	

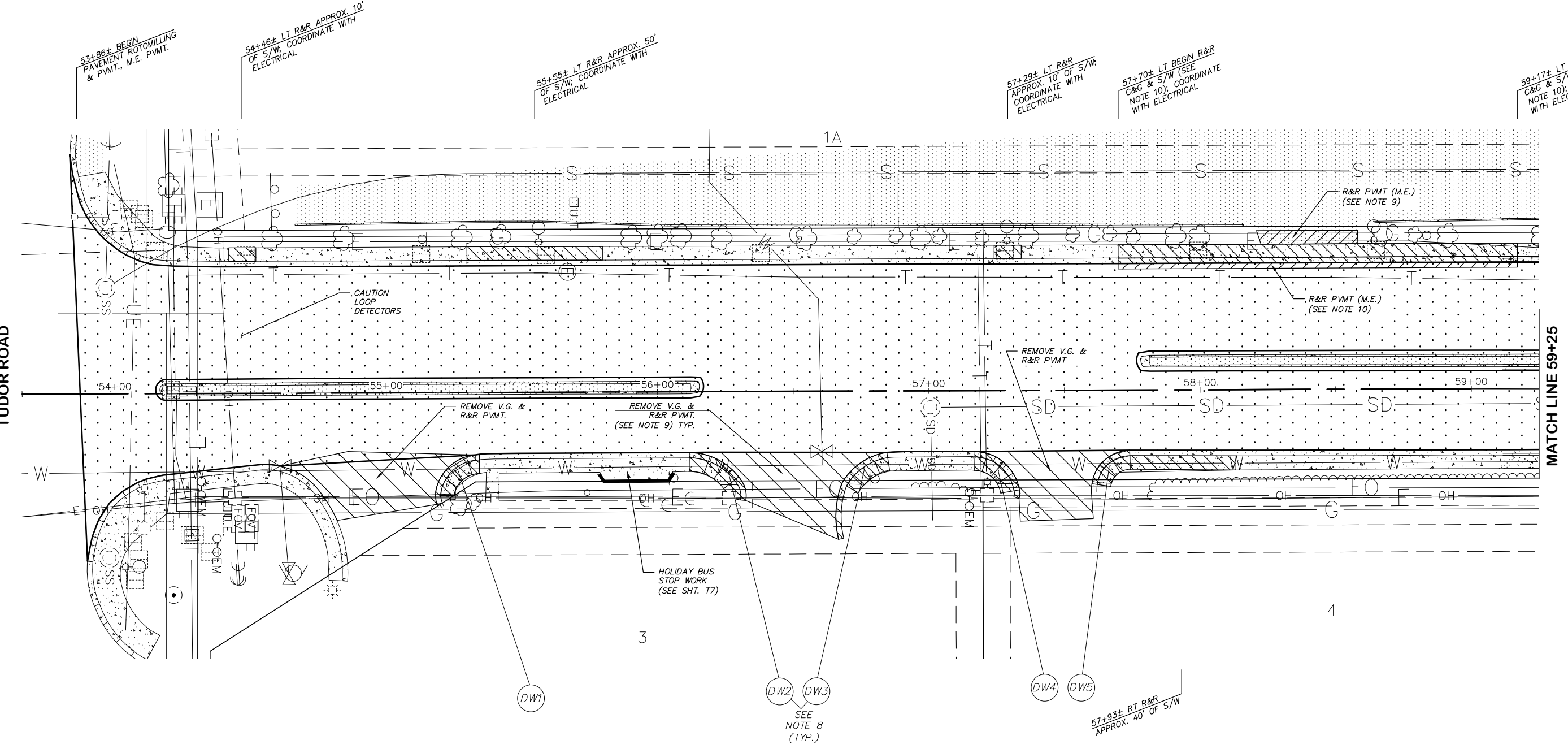
ADJUST VALVE BOX TABLE	
STA/OFFSET	
54+61± RT 27'±	
56+61± RT 22'±	
(SEE NOTE 7)	

ADJUST SDMH TABLE	
STA/OFFSET	
55+67± LT 44'±	
57+01± RT 6'±	
(SEE NOTES 3 & 4)	

ADJUST SANITARY SEWER MANHOLE RING TABLE	
STA/OFFSET	
53+97± LT 39'±	
(SEE NOTE 6)	

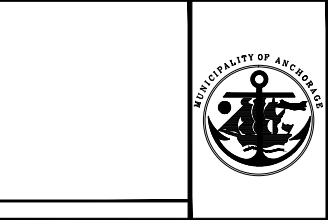
- NOTES:
- SIDEWALK AND C&G R&R LOCATIONS ARE APPROXIMATE AND WILL BE DETERMINED BY THE ENGINEER. SIDEWALK SHALL BE 4" THICK UNLESS OTHERWISE SPECIFIED ON PLANS OR AS DIRECTED BY THE ENGINEER.
 - MOST AREAS OF SIDEWALK REMOVAL & REPLACEMENT INCLUDE ELECTRICAL IMPROVEMENTS. COORDINATE EXTENTS OF SIDEWALK WORK WITH ELECTRICAL CONTRACTOR.
 - ADJUST STORM DRAIN MANHOLE I.A.W. M.A.S.S. 55.08 "ADJUST STORM DRAIN MANHOLE RING" AND STD. DTL. 55-10.
 - ADJUSTING OTHER MANHOLES (TELEPHONE, ELECTRICAL) TO FINISH GRADE SHALL BE PAID FOR AS BID ITEM 55.08 "ADJUST STORM DRAIN MANHOLE RING" AND NO ADDITIONAL PAYMENT SHALL BE MADE.
 - AWWU WILL SUPPLY WATER VALVE LIDS, DUST PANS, AND SANITARY SEWER MANHOLE COVERS ON AN AS NEEDED BASIS DETERMINED BY A PRECONSTRUCTION INSPECTION. INSTALL PER M.A.S.S. AND AS DIRECTED BY THE ENGINEER. WORK SHALL BE INCIDENTAL TO M.A.S.S. 60.20 "ADJUST

- VALVE BOX TO FINISH GRADE" AND 50.19 "ADJUST SANITARY SEWER MANHOLE RING" AND NO SEPARATE PAYMENT SHALL BE MADE.
- ADJUST SANITARY SEWER MANHOLE RING I.A.W. "50.19 ADJUST SANITARY SEWER MANHOLE RING TO FINISH GRADE" AND STD. DTL. 50-5.
- ADJUST VALVE BOX I.A.W. M.A.S.S. 60.20 "ADJUST VALVE BOX TO FINISH GRADE" AND STD. DTL. 60-3.
- CONSTRUCT DRIVEWAY RAMP WITHOUT DETECTABLE WARNING PANEL I.A.W. SHEETS D2 & D3 AND AS DIRECTED BY THE ENGINEER. WORK SHALL BE PAID UNDER BID ITEM "REMOVE AND REPLACE CURB RAMP" AND NO ADDITIONAL PAYMENT SHALL BE MADE.
- R&R PAVEMENT FOR DRIVEWAYS SHALL BE I.A.W. DETAIL G, SHEET D1.
- R&R C&G & S/W TO IMPROVE DRAINAGE AS DIRECTED BY THE ENGINEER. PROVIDE POSITIVE DRAINAGE (1/8" SLOPE=0.77%). R&R PVMT. IN STREET I.A.W. DETAIL D, SHEET D1.



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DESIGN:	FIELD BOOKS	BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REVI	DATE	DESCRIPTION	BY
STAKING:					BASE	R&M		TELEPHONE	R&M					
ASBUILT:					TOPOGRAPHY	R&M		ELECTRIC	R&M					
CONTRACTOR:					PROFILE	R&M		CABLE TV	R&M					
INSPECTOR:					SANITARY SEWER	R&M		TRAFFIC SIGNAL	R&M					
					STORM SEWER	R&M		DESIGN	MOA	P.J.L.				
					WATER	R&M		QUANTITIES	MOA	P.J.L.				
					GAS	R&M		MUN. FINAL CHECK						
CONSTRUCTION RECORD					VERTICAL DATUM					PLAN CHECK				
										REVISIONS				



PUBLIC WORKS DEPARTMENT

10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH. A-B

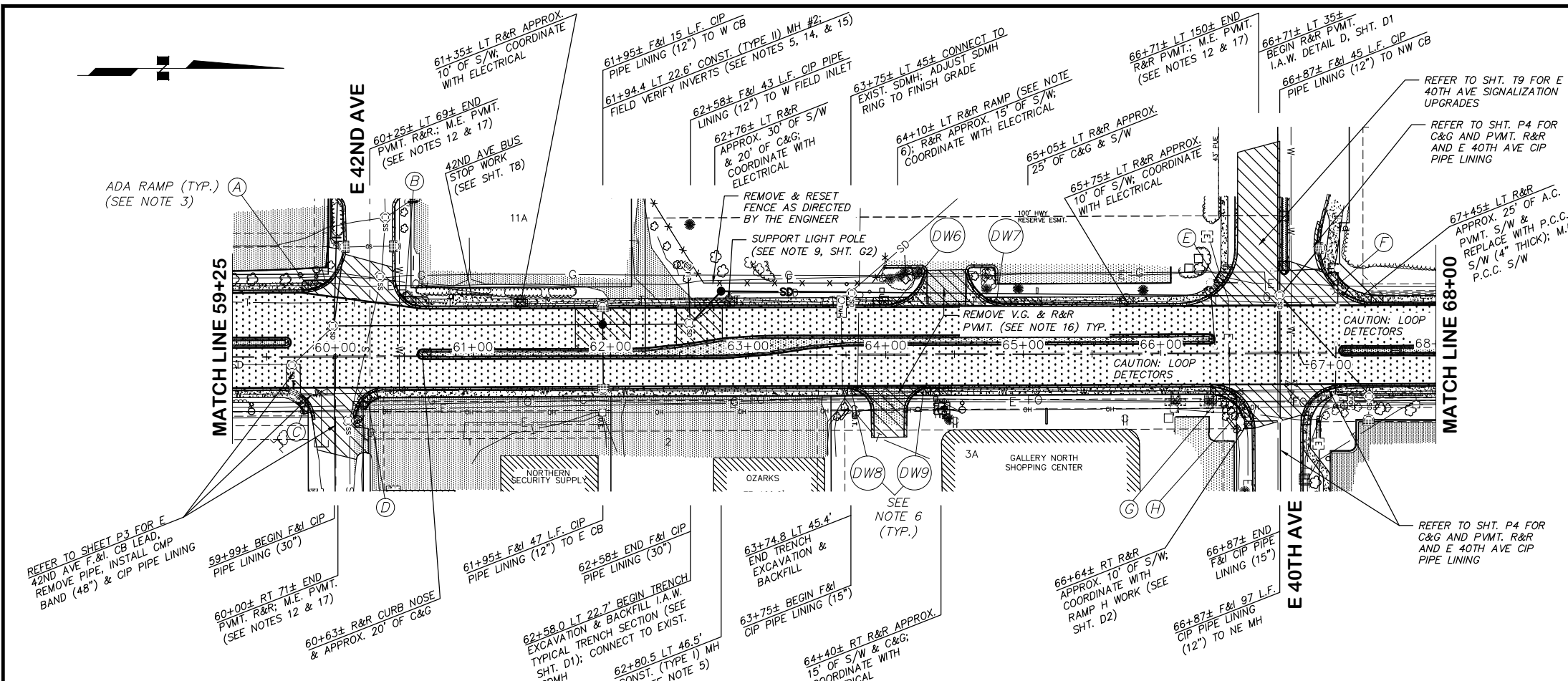
OLD SEWARD HIGHWAY STREET & STORM IMPROVEMENTS

B.O.P. (TUDOR ROAD) TO STA 59+25

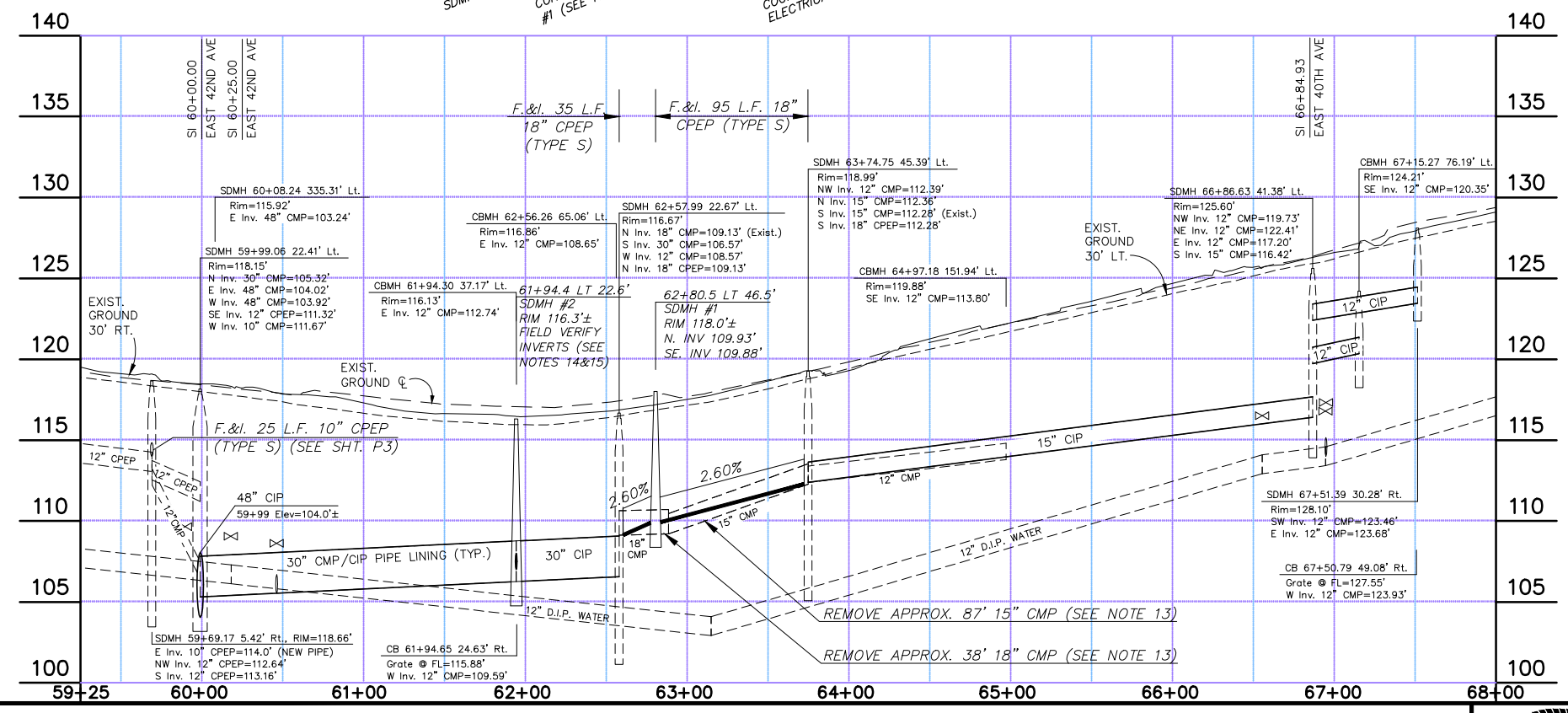
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DATE JAN 2013 GRID: SW1631, SW1632, SW1731, SW1732 SHEET **P1** of **P5**

G:\Public Works\PM\PA\10-13 Old Seward Hwy Surface Rehab - 34th Ave to Tudor Rd\Design\ENG\dwg\10-13 Street-Storm.dwg



- NOTES:
- F&I CIP PIPE LINING I.A.W. SPECIAL PROVISIONS.
 - SIDEWALK AND C&G R&R LOCATIONS ARE APPROXIMATE AND WILL BE DETERMINED BY THE ENGINEER. SIDEWALK SHALL BE 4" THICK UNLESS OTHERWISE SPECIFIED ON PLANS OR AS DIRECTED BY THE ENGINEER.
 - SEE SHEET D2 FOR ADA RAMP DETAILS.
 - MOST AREAS OF SIDEWALK REMOVAL & REPLACEMENT INCLUDE ELECTRICAL IMPROVEMENTS. COORDINATE EXTENTS OF SIDEWALK WORK WITH ELECTRICAL CONTRACTOR.
 - PRIOR TO SETTING SDMHs #1 & #2, CONTRACTOR SHALL REMOVE UNSUITABLE MATERIAL AND REPLACE IT WITH FOUNDATION BACKFILL (TYPE II) AS DIRECTED BY THE ENGINEER. EXCAVATION OF UNSUITABLE MATERIAL SHALL BE PAID FOR UNDER 20.10 "UNUSABLE EXCAVATION" AND NO ADDITIONAL PAYMENT SHALL BE MADE.
 - CONSTRUCT DRIVEWAY RAMP WITHOUT DETECTABLE WARNING PANEL I.A.W. SHEETS D2 & D3 AND AS DIRECTED BY THE ENGINEER. WORK SHALL BE PAID UNDER BID ITEM "REMOVE AND REPLACE CURB RAMP" AND NO ADDITIONAL PAYMENT SHALL BE MADE.
 - AWWU WILL SUPPLY WATER VALVE LIDS, DUST PANS, AND SANITARY SEWER MANHOLE COVERS ON AN AS NEEDED BASIS DETERMINED BY A PRECONSTRUCTION INSPECTION. INSTALL PER M.A.S.S. AND AS DIRECTED BY THE ENGINEER. WORK SHALL BE INCIDENTAL TO M.A.S.S. 60.20 "ADJUST VALVE BOX TO FINISH GRADE" AND 50.19 "ADJUST SANITARY SEWER MANHOLE RING" AND NO SEPARATE PAYMENT SHALL BE MADE.
 - ADJUST SANITARY SEWER MANHOLE RING I.A.W. "50.19 ADJUST SANITARY SEWER MANHOLE RING TO FINISH GRADE" AND STD. DTL. 50-5.
 - ADJUST STORM DRAIN MANHOLE I.A.W. M.A.S.S. 55.08 "ADJUST STORM DRAIN MANHOLE RING" AND STD. DTL. 55-10.
 - ADJUSTING OTHER MANHOLES (TELEPHONE, ELECTRICAL) TO FINISH GRADE SHALL BE PAID FOR AS BID ITEM 55.08 "ADJUST STORM DRAIN MANHOLE RING" AND NO ADDITIONAL PAYMENT SHALL BE MADE.
 - ADJUST VALVE BOX I.A.W. M.A.S.S. 60.20 "ADJUST VALVE BOX TO FINISH GRADE" AND STD. DTL. 60-3.
 - TRANSVERSE A.C. JOINTS SHALL BE SKEWED BETWEEN 15 AND 25 DEGREES OFF PERPENDICULAR TO CENTERLINE PER M.A.S.S. SECTION 40.06 ARTICLE 6.5 J.
 - REMOVE AND DISPOSE OF STORM PIPE I.A.W. M.A.S.S. 70.07 "REMOVE PIPE". REMOVAL OF THAW PIPES, CONDUIT, AND WIRE IS INCIDENTAL TO THIS CONTRACT, AND NO SEPARATE PAYMENT SHALL BE MADE. AT STA 62+89± THE 15" STORM CMP TRANSITIONS TO 18" CMP (REFER TO PROFILE FOR PIPE REMOVAL CALLOUT).
 - FIELD VERIFY PIPE LOCATIONS/INVERT ELEVATIONS. CORE DRILL PIPE HOLES IN FIELD I.A.W. M.A.S.S. OR PROVIDE INVERT ELEVATION INFORMATION TO PRECAST CONCRETE MANHOLE SUPPLIER. CONNECTING EXISTING 30" STORM DRAIN PIPE AND CB LEADS TO NEW SDMH#2 SHALL BE PAID FOR UNDER "CONNECT TO EXISTING STORM DRAIN MANHOLE" (QTY=4) AND NO ADDITIONAL PAYMENT SHALL BE MADE.
 - SDMH#2 RIM ELEV. (116.3±) IS APPROXIMATE. SET RIM ELEV. BASED ON NEW PVMT. SURFACE ELEV. AND I.A.W. M.A.S.S. STD. DTL. 55-10. EXCAVATION AND BACKFILL FOR SDMH#2 SHALL BE I.A.W. DETAIL E, SHEET D1 AND PAID UNDER THE FOLLOWING M.A.S.S. BID ITEMS:
 - 20.10 UNUSABLE EXCAVATION
 - 20.19 FOUNDATION BACKFILL (TYPE II)
 - 20.15 FURNISH TRENCH BACKFILL (TYPE II)
 - 20.21 CLASSIFIED FILL & BACKFILL (TYPE II-A)
 AND NO ADDITIONAL PAYMENT SHALL BE MADE.
 - REMOVE & REPLACE PAVEMENT FOR DRIVEWAYS SHALL BE I.A.W. DETAIL G, SHEET D1.
 - REMOVE & REPLACE PAVEMENT FOR SIDE STREETS SHALL BE I.A.W. DETAIL D, SHT. D1.



ADJUST VALVE BOX TABLE	ADJUST SDMH TABLE	ADJUST SANITARY SEWER MANHOLE RING TABLE
STA/OFFSET	STA/OFFSET	STA/OFFSET
59+90± RT 26'±	59+69± RT 5'±	60+33± LT 57'±
60+18± RT 27'±	59+99± LT 22'±	60+10± RT 47'±
66+55± RT 28'±	62+58± LT 23'±	
66+95± RT 22'±	63+75± LT 45'±	
66+95± RT 32'±	66+87± LT 41'±	
(SEE NOTE 11)	(SEE NOTES 9 & 10)	(SEE NOTE 8)

FIELD BOOKS DESIGN: MOA Book 3576, Pages 1 - 73 STAKING: ASBUILT: CONTRACTOR: INSPECTOR:	BM NO. GAAB 93 LOCATION MOA 1988 Benchmark Book, D-54 ELEV. 113.95' BASIS OF DATUM: 1972 N.G.S. ADJUSTED DATUM	DATA BASE TOPOGRAPHY PROFILE SANITARY SEWER STORM SEWER WATER GAS	DRAWN BY R&M R&M R&M R&M R&M R&M R&M	CHECKED BY R&M R&M R&M R&M R&M R&M R&M	DATA TELEPHONE ELECTRIC CABLE TV TRAFFIC SIGNAL DESIGN QUANTITIES MUN. FINAL CHECK	DRAWN BY R&M R&M R&M R&M R&M R&M R&M	CHECKED BY R&M R&M R&M R&M R&M R&M R&M	REV. DATE DESCRIPTION BY	REVISIONS	SEAL STATE OF ALASKA 49th PAUL J. LAFRANCE No. CE 10993 1/9/13 REGISTERED PROFESSIONAL ENGINEER	PUBLIC WORKS DEPARTMENT 10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH. A-B OLD SEWARD HIGHWAY STREET & STORM IMPROVEMENTS STA 59+25 TO STA 68+00 SCALE: HOR. 1"=50' VER. 1"=5' DATE JAN 2013 ACCT. NO. SHEET P2 of P5
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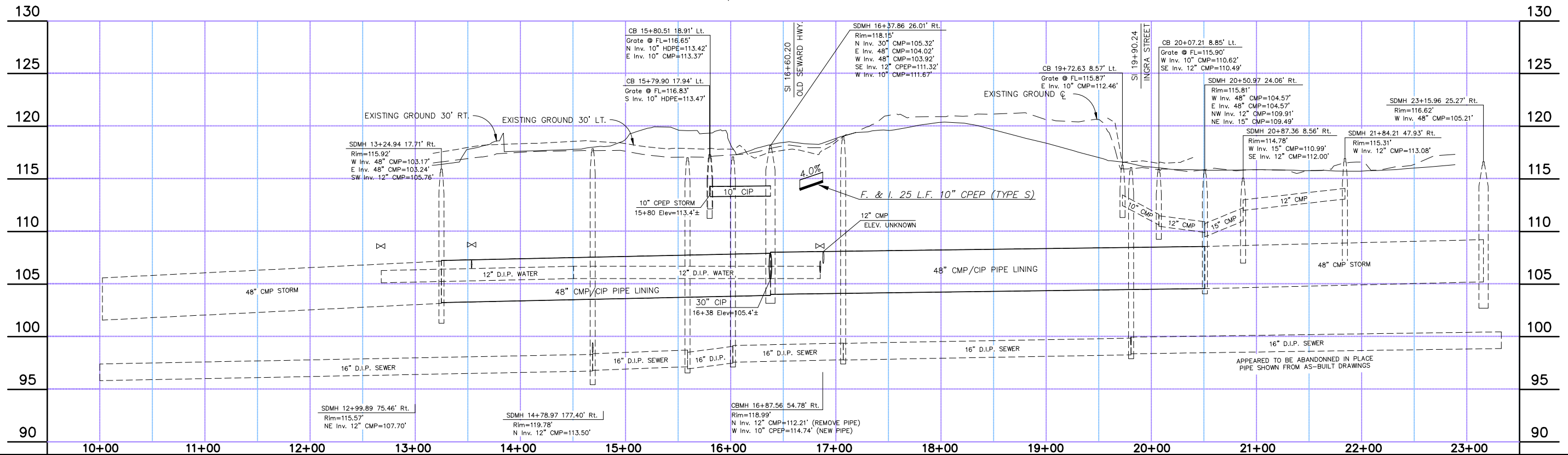
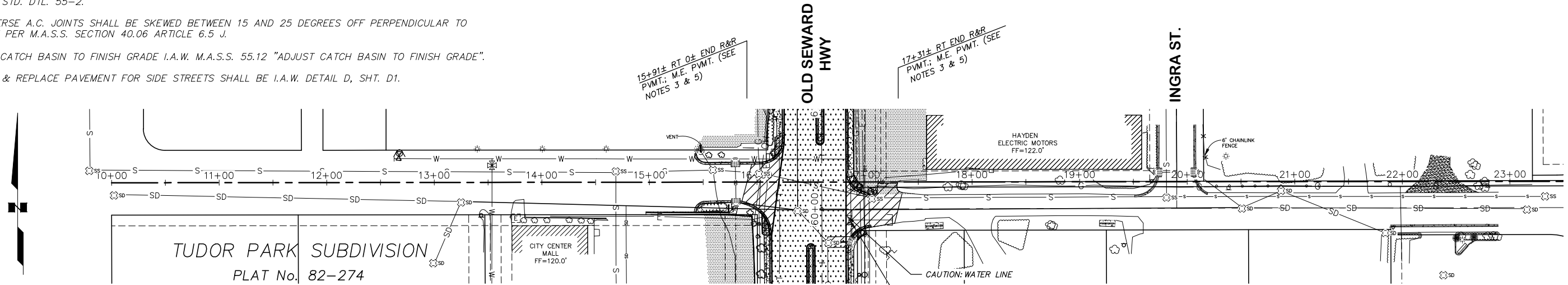
NOTES:
 1. F&I CIP PIPE LINING I.A.W. SPECIAL PROVISIONS.

2. THE 12" CB LEAD TO BE REMOVED, CONNECTS DIRECTLY TO THE 48" CMP AT STA 16+88±. REMOVE PIPE I.A.W. M.A.S.S. 70.07. F. & I. CORRUGATED METAL PIPE BAND (48") I.A.W. SPECIAL PROVISIONS SECTION 55.29 AND STD. DTL. 55-2.

3. TRANSVERSE A.C. JOINTS SHALL BE SKEWED BETWEEN 15 AND 25 DEGREES OFF PERPENDICULAR TO CENTERLINE PER M.A.S.S. SECTION 40.06 ARTICLE 6.5 J.

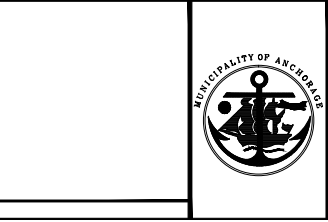
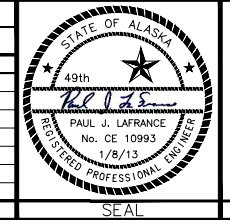
4. ADJUST CATCH BASIN TO FINISH GRADE I.A.W. M.A.S.S. 55.12 "ADJUST CATCH BASIN TO FINISH GRADE".

5. REMOVE & REPLACE PAVEMENT FOR SIDE STREETS SHALL BE I.A.W. DETAIL D, SHT. D1.



G:\Public Works\PM\PA\10-13 Old Seward Hwy Surface Rehab - 34th Ave to Tudor Rd\Design\ENG\dwg\10-13 Street-Storm.dwg

FIELD BOOKS	BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REVI	DATE	DESCRIPTION	BY
DESIGN: MOA Book 3576, Pages 1 - 73	GAAB 93	MOA 1988 Benchmark Book, D-54	113.95'	BASE	R&M		TELEPHONE	R&M					
STAKING:				TOPOGRAPHY	R&M		ELECTRIC	R&M					
ASBUILT:				PROFILE	R&M		CABLE TV	R&M					
CONTRACTOR:				SANITARY SEWER	R&M		TRAFFIC SIGNAL	R&M					
INSPECTOR:				STORM SEWER	R&M		DESIGN	MOA	P.J.L.				
CONSTRUCTION RECORD				WATER	R&M		QUANTITIES	MOA	P.J.L.				
				GAS	R&M		MUN. FINAL CHECK						



PUBLIC WORKS DEPARTMENT

10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH. A-B

E 42ND AVENUE STREET & STORM IMPROVEMENTS

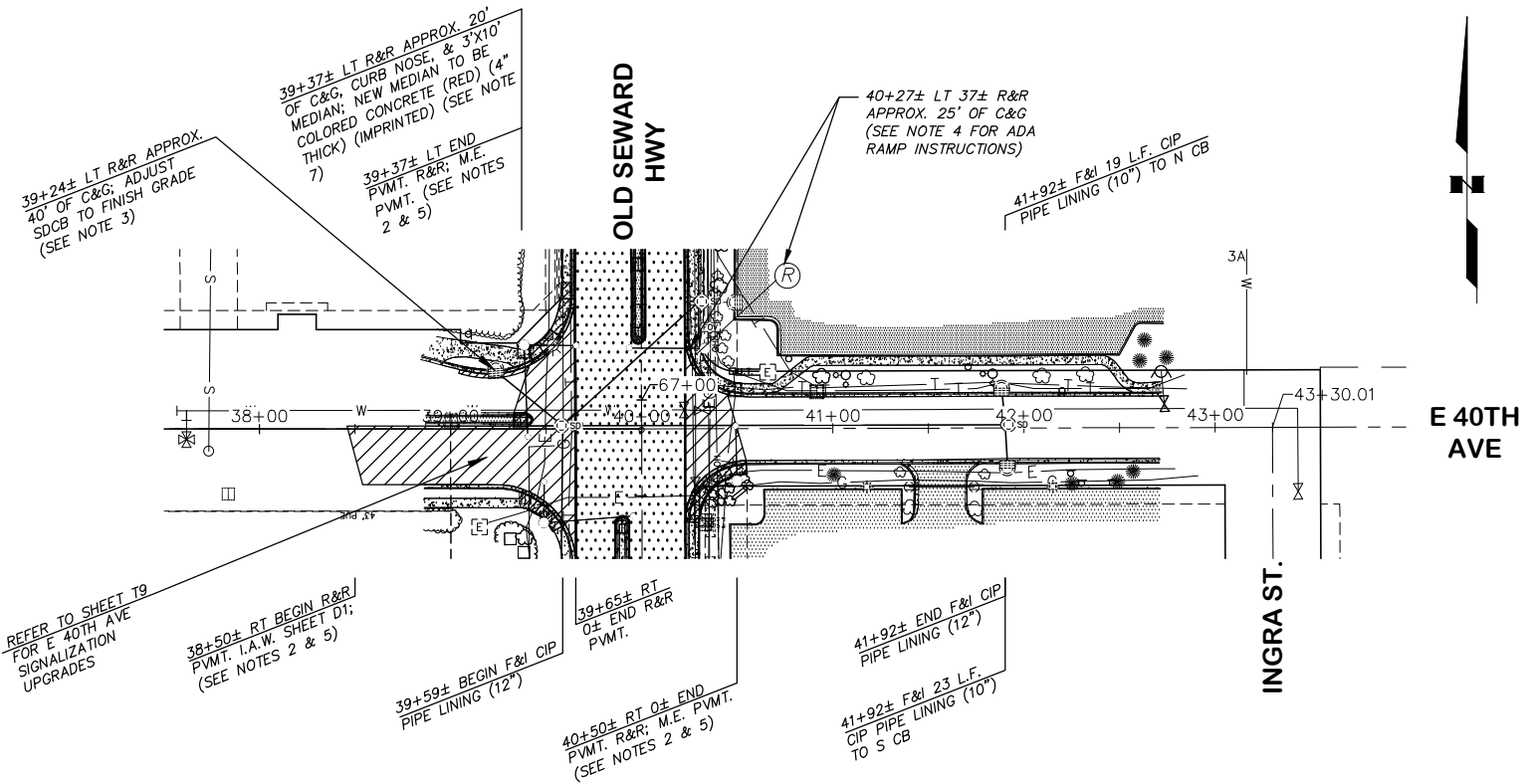
34TH AVENUE TO TUDOR ROAD

SCALE: HOR. 1"=50'
VER. 1"=5'

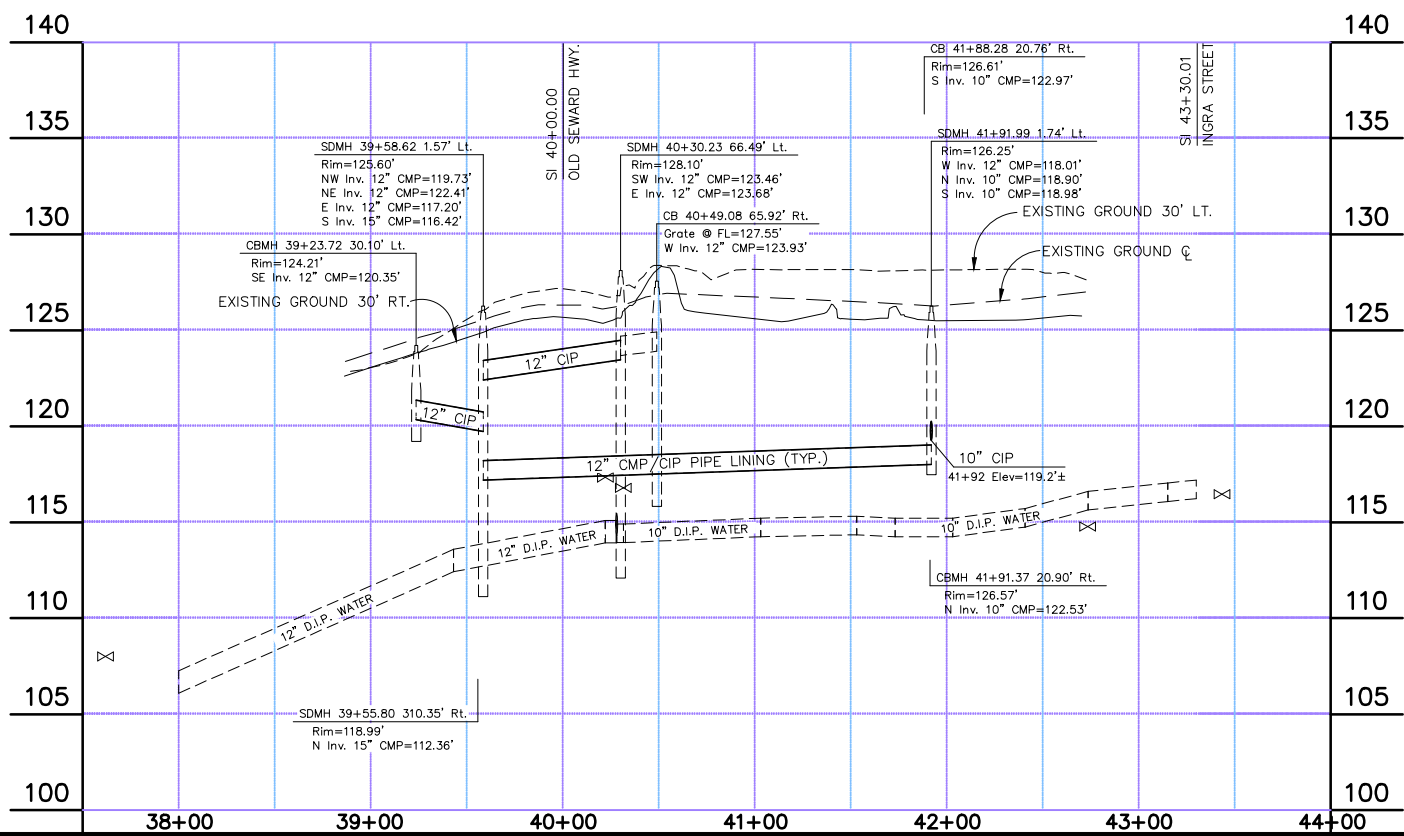
DATE JAN 2013
ACCT. NO.

GRID: SW1631, SW1632, SW1731, SW1732

SHEET **P3** of **P5**

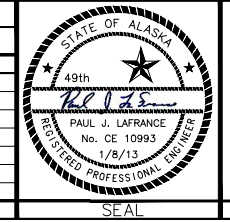


- NOTES:
1. F&I CIP PIPE LINING I.A.W. SPECIAL PROVISIONS.
 2. TRANSVERSE A.C. JOINTS SHALL BE SKEWED BETWEEN 15 AND 25 DEGREES OFF PERPENDICULAR TO CENTERLINE PER M.A.S.S. SECTION 40.06 ARTICLE 6.5 J.
 3. ADJUST CATCH BASIN TO FINISH GRADE I.A.W. M.A.S.S. 55.12 "ADJUST CATCH BASIN TO FINISH GRADE".
 4. IF C&G IS ATTACHED TO ADA RAMP (MONOLITHIC POUR), R&R PARALLEL RAMP & RAMP LANDINGS AS DIRECTED BY THE ENGINEER & I.A.W. SHEET D2. ADJUST JUNCTION BOX TO GRADE. IF C&G AND ADA RAMP ARE SEPARATE, R&R C&G ONLY.
 5. REMOVE & REPLACE PAVEMENT FOR SIDE STREETS SHALL BE I.A.W. DETAIL D, SHT. D1.
 6. MOST AREAS OF SIDEWALK REMOVAL & REPLACEMENT INCLUDE ELECTRICAL IMPROVEMENTS. COORDINATE EXTENTS OF SIDEWALK WORK WITH ELECTRICAL CONTRACTOR.
 7. MEDIAN AND CURB NOSE REMOVAL WILL BE PAID UNDER 20.07 "REMOVE SIDEWALK" AND NO SEPARATE PAYMENT SHALL BE MADE (TYPICAL FOR ALL SHEETS).



G:\Public Works\PM\PA\10-13 Old Seward Hwy Surface Rehab - 34th Ave to Tudor Rd\Design\ENG\dwg\10-13 Street-Storm.dwg

FIELD BOOKS	BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REVI	DATE	BY	REV	DATE	DESCRIPTION	BY
DESIGN: MOA Book 3576, Pages 1 - 73	GAAB 93	MOA 1988 Benchmark Book, D-54	113.95'	BASE	R&M		TELEPHONE	R&M								
STAKING:				TOPOGRAPHY	R&M		ELECTRIC	R&M								
ASBUILT:				PROFILE	R&M		CABLE TV	R&M								
CONTRACTOR:				SANITARY SEWER	R&M		TRAFFIC SIGNAL	R&M								
INSPECTOR:				STORM SEWER	R&M		DESIGN	MOA	P.J.L.							
				WATER	R&M		QUANTITIES	MOA	P.J.L.							
				GAS	R&M		MUN. FINAL CHECK									



PUBLIC WORKS DEPARTMENT

10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH. A-B

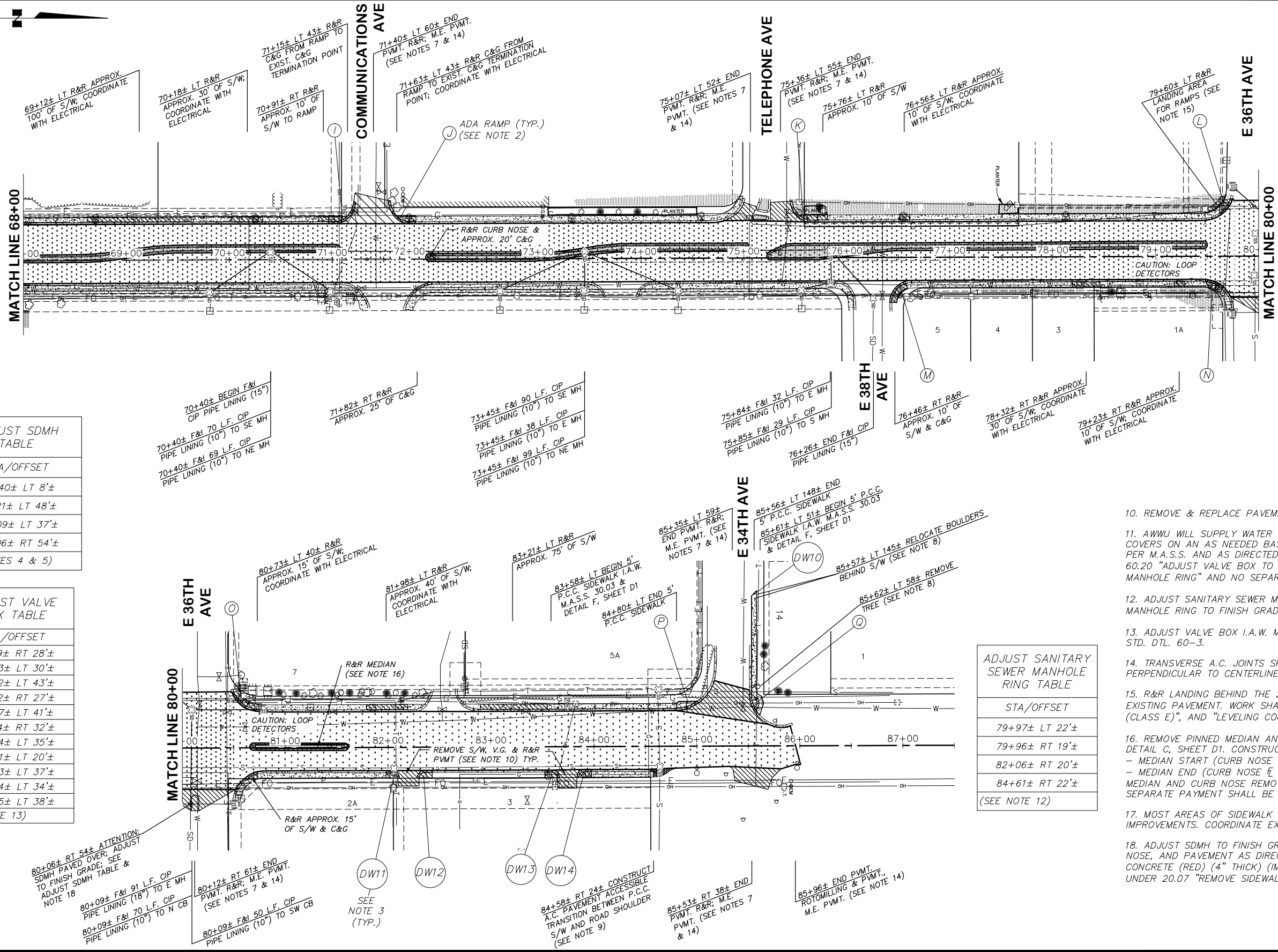
**E 40TH AVENUE
STREET & STORM IMPROVEMENTS**

34TH AVENUE TO TUDOR ROAD

SCALE: HOR. 1"=50'
VER. 1"=5'

DATE JAN 2013 GRID: SW1631, SW1632, SW1731, SW1732 SHEET **P4** of **P5**

G:\Public Works\PM\10-13 Old Seward Hwy Surface Rehab - 34th Ave to Tudor Rd\Design\ENG\10-13 Street-Storm.dwg



- NOTES:
1. F&I CIP PIPE LINING I.A.W. SPECIAL PROVISIONS.
 2. SEE SHEET D2 FOR ADA RAMP DETAILS.
 3. CONSTRUCT DRIVEWAY RAMP WITHOUT DETECTABLE WARNING PANEL I.A.W. SHEETS D2 & D3 AND AS DIRECTED BY THE ENGINEER. WORK SHALL BE PAID UNDER BID ITEM "REMOVE AND REPLACE CURB RAMP" AND NO ADDITIONAL PAYMENT SHALL BE MADE.
 4. ADJUST STORM DRAIN MANHOLE I.A.W. M.A.S.S. 55.08 "ADJUST STORM DRAIN MANHOLE RING" AND STD. DTL. 55-10.
 5. ADJUSTING OTHER MANHOLES (TELEPHONE, ELECTRICAL) TO FINISH GRADE SHALL BE PAID FOR AS BID ITEM 55.08 "ADJUST STORM DRAIN MANHOLE RING" AND NO ADDITIONAL PAYMENT SHALL BE MADE.
 6. SIDEWALK AND C&G R&R LOCATIONS ARE APPROXIMATE AND WILL BE DETERMINED BY THE ENGINEER. SIDEWALK SHALL BE 4" THICK UNLESS OTHERWISE SPECIFIED ON PLANS OR AS DIRECTED BY THE ENGINEER.
 7. REMOVE & REPLACE PAVEMENT FOR SIDE STREETS SHALL BE I.A.W. DETAIL D, SHT. D1.
 8. RELOCATE BOULDERS AND REMOVE TREE (AS DIRECTED BY THE ENGINEER) IS INCIDENTAL TO THIS CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
 9. CONSTRUCT A.C. PAVEMENT ACCESSIBLE TRANSITION BETWEEN P.C.C. SIDEWALK AND ROAD SHOULDER AS DIRECTED BY THE ENGINEER. REMOVE APPROXIMATELY 15' OF C&G. CONSTRUCT TRANSITION AT 2% CROSS SLOPE TO PROVIDE POSITIVE DRAINAGE AWAY FROM ROADWAY. TRANSITION SECTION SHALL CONSIST OF:
 - 2" A.C. PAVEMENT (CLASS E)
 - 2" LEVELING COURSE
 - 18" CLASSIFIED FILL & BACKFILL (TYPE IIA)
 - P.C.C. CURB AND GUTTER (CONSTRUCT TRANSITION FROM TYPE 1 TO TYPE 1A C&G)
 WORK SHALL BE PAID UNDER M.A.S.S. 20.08 "REMOVE CURB AND GUTTER" AND THE BID ITEMS LISTED ABOVE AND NO ADDITIONAL PAYMENT SHALL BE MADE.
 10. REMOVE & REPLACE PAVEMENT FOR DRIVEWAYS SHALL BE I.A.W. DETAIL G, SHT. D1.
 11. AWWU WILL SUPPLY WATER VALVE LIDS, DUST PANS, AND SANITARY SEWER MANHOLE COVERS ON AN AS NEEDED BASIS DETERMINED BY A PRECONSTRUCTION INSPECTION. INSTALL PER M.A.S.S. AND AS DIRECTED BY THE ENGINEER. WORK SHALL BE INCIDENTAL TO M.A.S.S. 60.20 "ADJUST VALVE BOX TO FINISH GRADE" AND 50.19 "ADJUST SANITARY SEWER MANHOLE RING" AND NO SEPARATE PAYMENT SHALL BE MADE.
 12. ADJUST SANITARY SEWER MANHOLE RING I.A.W. "50.19 ADJUST SANITARY SEWER MANHOLE RING TO FINISH GRADE" AND STD. DTL. 50-5.
 13. ADJUST VALVE BOX I.A.W. M.A.S.S. 60.20 "ADJUST VALVE BOX TO FINISH GRADE" AND STD. DTL. 60-3.
 14. TRANSVERSE A.C. JOINTS SHALL BE SKEWED BETWEEN 15 AND 25 DEGREES OFF PERPENDICULAR TO CENTERLINE PER M.A.S.S. SECTION 40.06 ARTICLE 6.5 J.
 15. R&R LANDING BEHIND THE 2 ADA RAMP I.A.W. M.A.S.S. STD. DTL. 30-11. MATCH EXISTING PAVEMENT. WORK SHALL BE PAID UNDER "REMOVE PAVEMENT", "A.C. PAVEMENT (CLASS E)", AND "LEVELING COURSE" AND NO ADDITIONAL PAYMENT SHALL BE MADE.
 16. REMOVE PINNED MEDIAN AND REPLACE WITH CURB NOSE (2) AND 3' WIDE MEDIAN I.A.W. DETAIL C, SHEET D1. CONSTRUCT CURB NOSE I.A.W. STD. DTL. 30-4.
 - MEDIAN START (CURB NOSE [L] = 80+67.3 LT 1.1')
 - MEDIAN END (CURB NOSE [R] = 81+59.8 LT 0.7')
 MEDIAN AND CURB NOSE REMOVAL WILL BE PAID UNDER 20.07 "REMOVE SIDEWALK" AND NO SEPARATE PAYMENT SHALL BE MADE.
 17. MOST AREAS OF SIDEWALK REMOVAL & REPLACEMENT INCLUDE ELECTRICAL IMPROVEMENTS. COORDINATE EXTENTS OF SIDEWALK WORK WITH ELECTRICAL CONTRACTOR.
 18. ADJUST SDMH TO FINISH GRADE. REMOVE AND REPLACE MEDIAN, CURB & GUTTER, CURB NOSE, AND PAVEMENT AS DIRECTED BY THE ENGINEER. NEW MEDIAN TO BE COLORED CONCRETE (RED) (4" THICK) (IMPRINTED). MEDIAN AND CURB NOSE REMOVAL WILL BE PAID UNDER 20.07 "REMOVE SIDEWALK" AND NO SEPARATE PAYMENT SHALL BE MADE.

ADJUST SDMH TABLE

STA/OFFSET
70+40± LT 8'±
71+21± LT 48'±
80+09± LT 37'±
80+06± RT 54'±

(SEE NOTES 4 & 5)

ADJUST VALVE BOX TABLE

STA/OFFSET
71+29± RT 28'±
75+43± LT 30'±
75+42± LT 43'±
79+62± RT 27'±
80+17± LT 41'±
80+14± RT 32'±
80+44± LT 35'±
80+61± LT 20'±
82+53± LT 37'±
83+34± LT 34'±
85+45± LT 38'±

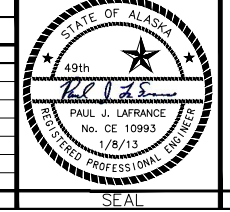
(SEE NOTE 13)

ADJUST SANITARY SEWER MANHOLE RING TABLE

STA/OFFSET
79+97± LT 22'±
79+96± RT 19'±
82+06± RT 20'±
84+61± RT 22'±

(SEE NOTE 12)

DESIGN:	STAKING:	ASBUILT:	CONTRACTOR:	INSPECTOR:	FIELD BOOKS	BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REVI	DATE	DESCRIPTION	BY	
									BASE			TELEPHONE							
									R&M			ELECTRIC							
									R&M			CABLE TV							
									R&M			TRAFFIC SIGNAL							
									R&M			DESIGN							
									R&M			QUANTITIES							
									R&M			MUN. FINAL CHECK							



PUBLIC WORKS DEPARTMENT

10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH. A-B

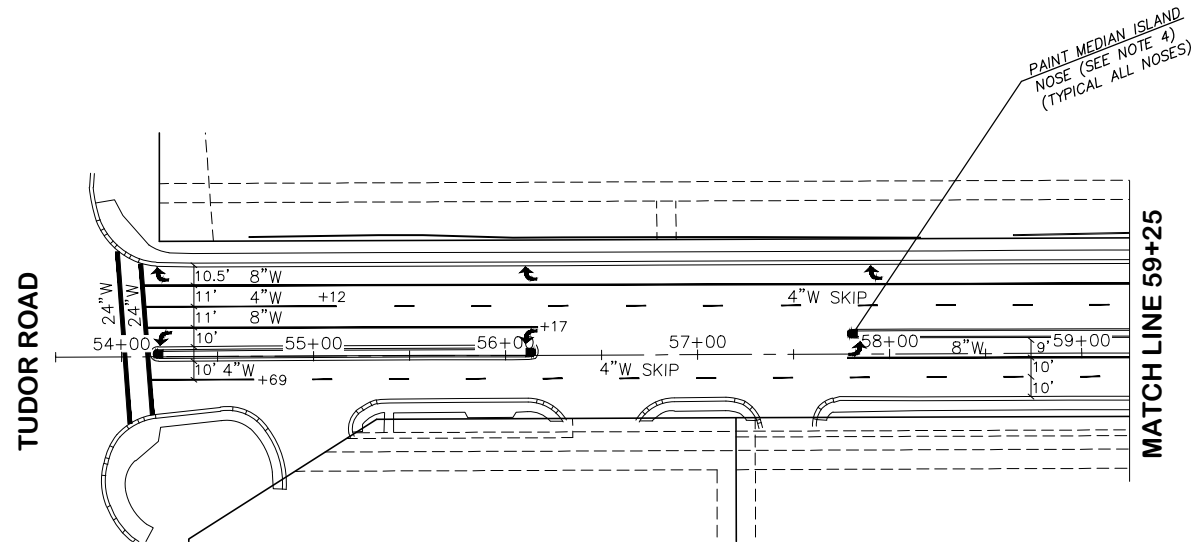
OLD SEWARD HIGHWAY STREET & STORM IMPROVEMENTS

STA 68+00 TO E.O.P. (E 34TH AVE)

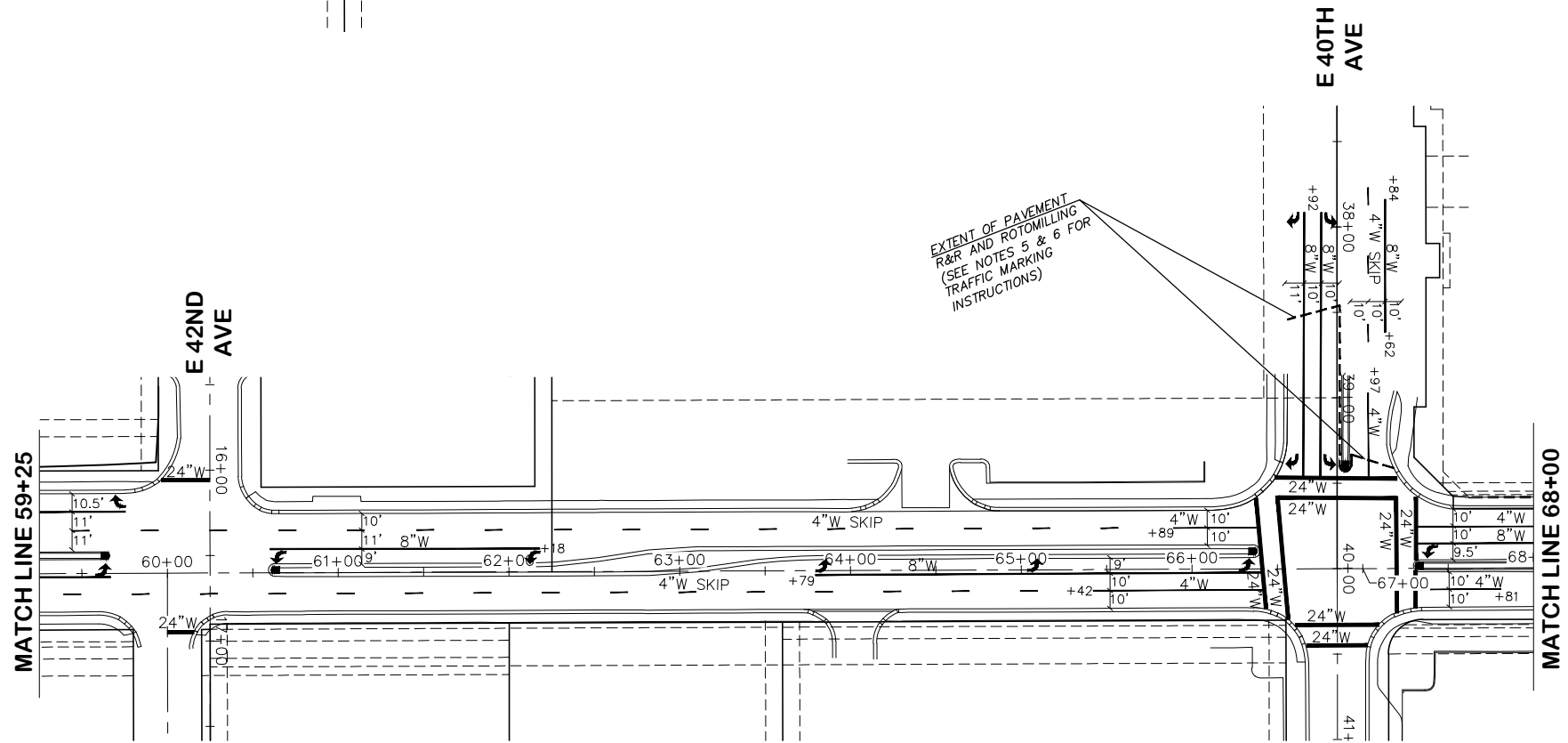
SCALE: 1"=50'

DATE JAN 2013 GRID: SW1631/SW1632 SW1731/SW1732 SHEET P5 of P5

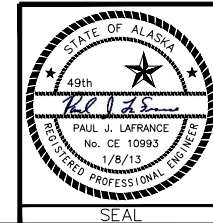
G:\Public Works\PM\PA\10-13 Old Seward Hwy Surface Rehab - 34th Ave to Tudor Rd\Design\ENG\dwg\10-13 Street-Sterm.dwg



- NOTES:
- DISTANCES SHOWN ARE TO CENTER OF SINGLE STRIPE, TO CENTER OF PAIR OF STRIPES, LIP OF GUTTER, OR TO FACE OF CURB FOR PIN-DOWN CURB UNLESS OTHERWISE NOTED.
 - LANE WIDTHS ARE BASED ON AERIAL PHOTOGRAPHY. CONTRACTOR SHALL NOTE WHERE EXISTING WIDTHS VARY FROM THOSE SHOWN ON THE DRAWINGS AND PROVIDE THAT INFORMATION TO THE ENGINEER PRIOR TO STRIPING FOR CLARIFICATION.
 - UNLESS OTHERWISE INDICATED, TRAFFIC MARKINGS SHALL BE INLAID, METHYL METHACRYLATE, 250 MIL OR AN APPROVED EQUAL.
 - PAINT TOP MEDIAN NOSE AND THE TOP FACE OF CURB OF ALL MEDIAN ISLAND NOSING YELLOW, A MINIMUM OF 5' BACK FROM THE FLOWLINE. PAINTED MEDIAN NOSING SHALL BE PAID UNDER M.A.S.S. 70.10 "TRAFFIC MARKINGS (PAINT 20 MILS) (WORDS & SYMBOLS)" AND NO ADDITIONAL PAYMENT SHALL BE MADE.
 - IN AREAS BEYOND THE EXTENT OF PAVEMENT REMOVAL & REPLACEMENT OR ROTOMILLING, REMOVE EXISTING TRAFFIC MARKINGS PER M.A.S.S. 70.10 AND INSTALL INLAID TRAFFIC MARKINGS (METHYL METHACRYLATE, 250 MIL) OR AS DIRECTED BY THE ENGINEER. REMOVAL OF EXISTING TRAFFIC MARKINGS IS INCIDENTAL TO 70.10 "INLAID TRAFFIC MARKINGS" AND NO ADDITIONAL PAYMENT SHALL BE MADE.
 - AT STA 38+10± RT, REMOVE (2) "ONLY" SYMBOLS AND THE THROUGH ARROW TRAFFIC MARKINGS. DO NOT REPLACE THESE TRAFFIC MARKINGS. REMOVAL OF EXISTING TRAFFIC MARKINGS IS INCIDENTAL TO 70.10 "INLAID TRAFFIC MARKINGS" AND NO ADDITIONAL PAYMENT SHALL BE MADE.



FIELD BOOKS	DATA:	DATA	DRAWN BY	CHECKED BY	REV	DATE	DESCRIPTION	BY
DESIGN: N/A	ALL DATA SHOWN WAS OBTAINED FROM THE MUNICIPALITY OF ANCHORAGE GEOGRAPHICAL INFORMATION SYSTEMS MAPPING. NO SURVEY WAS CONDUCTED FOR THIS PROJECT. CONTRACTOR SHALL FIELD VERIFY INFORMATION SHOWN ON DRAWINGS PRIOR TO BIDDING THE PROJECT.	DESIGN	P.J.L.	P.J.L.				
STAKING:		QUANTITIES						
ASBUILT:		MUN. FINAL CHECK						
CONTRACTOR:								
INSPECTOR:								
CONSTRUCTION RECORD	PLAN CHECK	REVISIONS						



PUBLIC WORKS DEPARTMENT

10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH. A

STRIPING PLAN

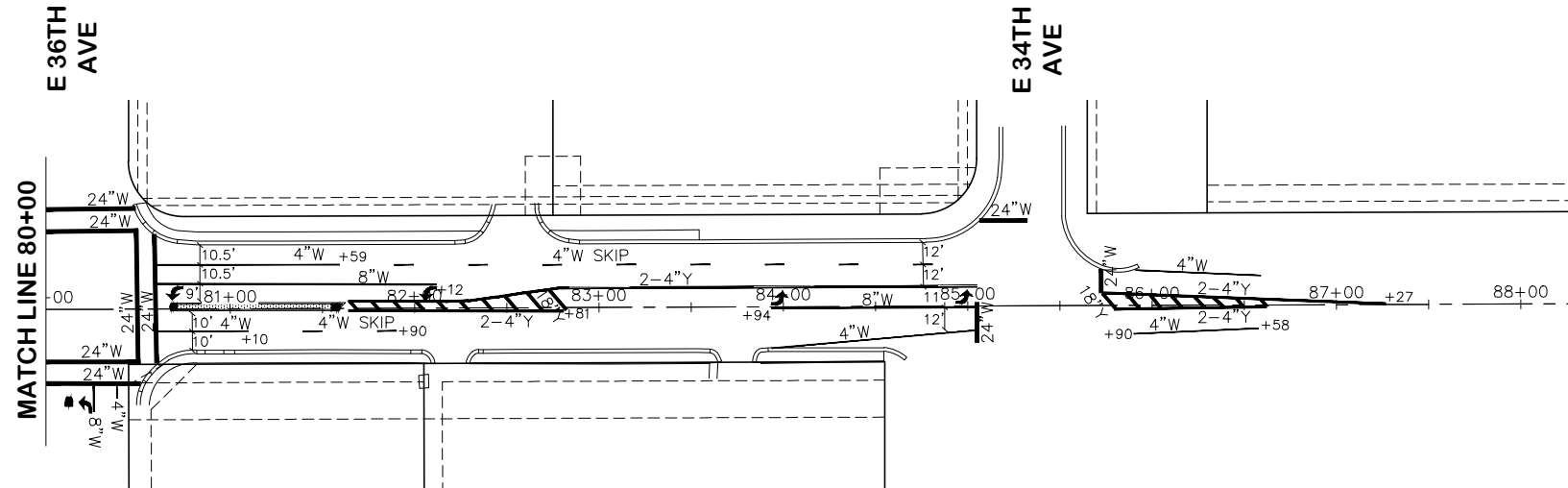
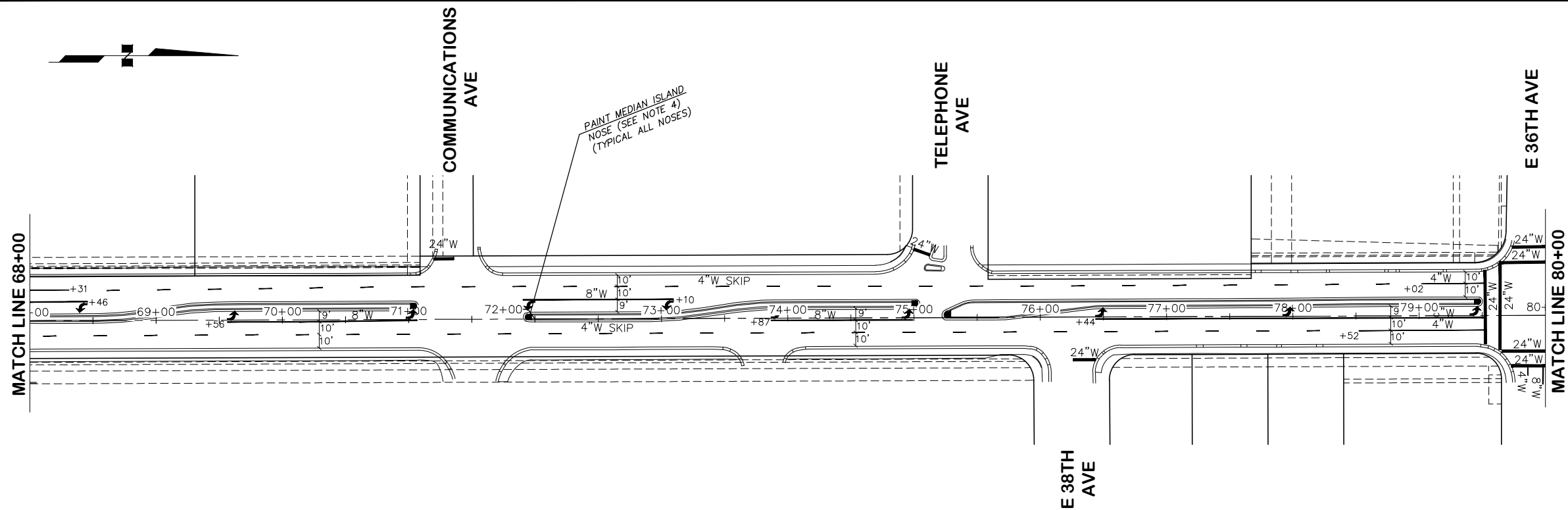
B.O.P. (TUDOR ROAD) TO STA 68+00

SCALE: 1" = 50'

DATE: JAN 2013 GRID: SW1631, SW1632, SW1731, SW1732 SHEET **S1** of **S2**

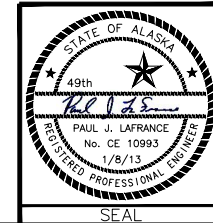
ACCT. NO.

G:\Public Works\PM\10-13 Old Seward Hwy Surface Rehab - 34th Ave to Tudor Rd\Design\ENG\dwg\10-13 Street-Sterrm.dwg



- NOTES:
- DISTANCES SHOWN ARE TO CENTER OF SINGLE STRIPE, TO CENTER OF PAIR OF STRIPES, LIP OF GUTTER, OR TO FACE OF CURB FOR PIN-DOWN CURB UNLESS OTHERWISE NOTED.
 - LANE WIDTHS ARE BASED ON AERIAL PHOTOGRAPHY. CONTRACTOR SHALL NOTE WHERE EXISTING WIDTHS VARY FROM THOSE SHOWN ON THE DRAWINGS AND PROVIDE THAT INFORMATION TO THE ENGINEER PRIOR TO STRIPING FOR CLARIFICATION.
 - UNLESS OTHERWISE INDICATED, TRAFFIC MARKINGS SHALL BE 250 MIL INLAID, METHYL METHACRYLATE OR AN APPROVED EQUAL.
 - PAINT TOP MEDIAN NOSE AND THE TOP FACE OF CURB OF ALL MEDIAN ISLAND NOSING YELLOW. A MINIMUM OF 5' BACK FROM THE FLOWLINE. PAINTED MEDIAN NOSING SHALL BE PAID UNDER M.A.S.S. 70.10 "TRAFFIC MARKINGS (PAINT 20 MILS) (WORDS & SYMBOLS)" AND NO ADDITIONAL PAYMENT SHALL BE MADE.

FIELD BOOKS	DATA:	DATA	DRAWN BY	CHECKED BY	REV	DATE	DESCRIPTION	BY
DESIGN: N/A	ALL DATA SHOWN WAS OBTAINED FROM THE MUNICIPALITY OF ANCHORAGE GEOGRAPHICAL INFORMATION SYSTEMS MAPPING. NO SURVEY WAS CONDUCTED FOR THIS PROJECT. CONTRACTOR SHALL FIELD VERIFY INFORMATION SHOWN ON DRAWINGS PRIOR TO BIDDING THE PROJECT.	DESIGN	P.J.L.					
STAKING:		QUANTITIES	P.J.L.					
ASBUILT:		MUN. FINAL CHECK						
CONTRACTOR:								
INSPECTOR:								
CONSTRUCTION RECORD	PLAN CHECK	REVISIONS						



PUBLIC WORKS DEPARTMENT

10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH. A

STRIPING PLAN

STA 68+00 TO E.O.P.

SCALE: 1" = 50'

DATE: JAN 2013 GRID: SW1631, SW1632, SW1731, SW1732 SHEET **S2** of **S2**

ACCT. NO.

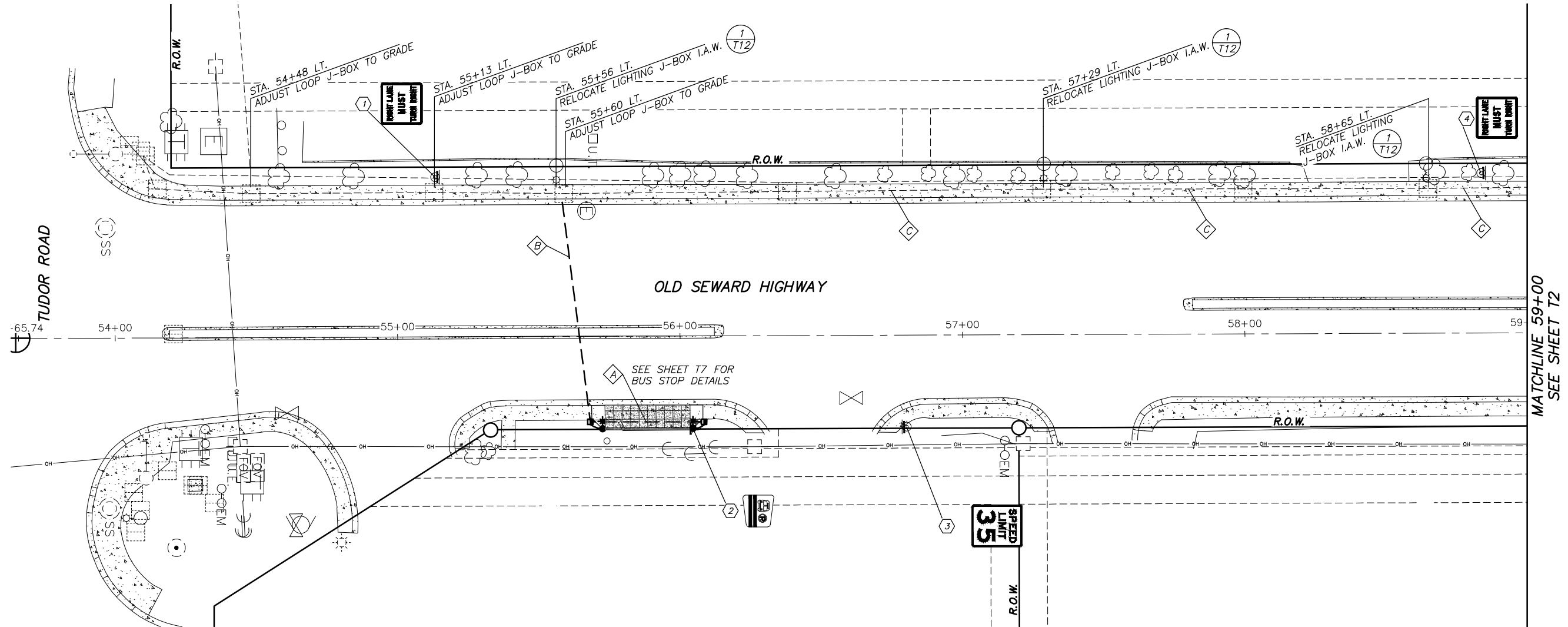
SIGNING NOTES:

1. (X) - SIGN POST NUMBER (REFERS TO SIGN POST NUMBER NOTES IN THE SIGN SUMMARY TABLES, SHEETS T10-T11).
2. SEE SHEET T10 FOR ADDITIONAL SIGNING NOTES.

ELECTRICAL NOTES:

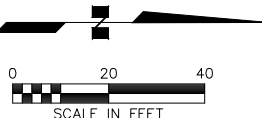
1. SEE CIVIL SHEETS FOR PAVEMENT, SIDEWALK, AND CURB&GUTTER REMOVAL AND REPLACEMENT DETAILS.
2. NEW ELECTROLIERS SHALL CONSIST OF 35 FT SLIP-BASE POLE; 18 FT LUMINAIRE ARM; 400 WATT, 480 VOLT HPS LUMINAIRE; AND PILE FOUNDATION.
3. CABLING BETWEEN AN ELECTROLIER AND THE ADJACENT JUNCTION BOX SHALL CONSIST OF 2-3C CABLES AND ONE BARE 1C GROUND, WITH THE SAME WIRE GAUGE AS THE REST OF THE CIRCUIT.
4. F&I NEW HANDHOLE COVERS TO REPLACE MISSING OR DAMAGED COVERS ON ELECTROLIERS AND SIGNAL POLES TO REMAIN IN SERVICE. THIS WORK IS INCIDENTAL AND NO SEPARATE PAYMENT SHALL BE MADE.
5. REMOVAL OF EXISTING J-BOXES AND CABLE SHALL BE PAID UNDER M.A.S.S. 80.08 "REMOVE JUNCTION BOX". CABLE REMOVAL IS INCIDENTAL TO THIS CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
6. CONDUIT THAT TERMINATES IN A REMOVED JUNCTION BOX THAT IS NOT EXTENDED TO ANOTHER J-BOX SHALL BE ABANDONED I.A.W. M.A.S.S. 80.07. THIS WORK IS INCIDENTAL AND NO SEPARATE PAYMENT SHALL BE MADE.

INTERCEPTING A CONDUIT RUN AND ROUTING TO NEW JUNCTION BOX IS INCIDENTAL TO M.A.S.S. SECTION 80.08 ITEMS. EXTENDING CONDUIT TO ACCOMMODATE RELOCATED J-BOXES WILL BE PAID UNDER M.A.S.S. SECTION 80.07.



CONDUIT NOTES:

- A 2" RMC: 1-3c#8 (B1)
- B 2" RMC: 2-3c#8 (B1,B2)
- C EX. 2": 2-3c#8 (B1,B2) REMOVE EXISTING LIGHTING CABLES.



FIELD BOOKS	BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REV	DATE	DESCRIPTION	BY
DESIGN: MOA Book 3576, Pages 1 - 73	GAAB 93	MOA 1988 Benchmark Book, D-54	113.95'	BASE	R&M		TELEPHONE	R&M					
STAKING:				TOPOGRAPHY	R&M		ELECTRIC	R&M					
ASBUILT:				PROFILE	R&M		CABLE TV	R&M					
CONTRACTOR:				SANITARY SEWER	R&M		TRAFFIC SIGNAL	R&M					
INSPECTOR:				STORM SEWER	R&M		DESIGN	USKH	WW				
				WATER	R&M		QUANTITIES	USKH	WW				
				GAS	R&M		MUN. FINAL CHECK						

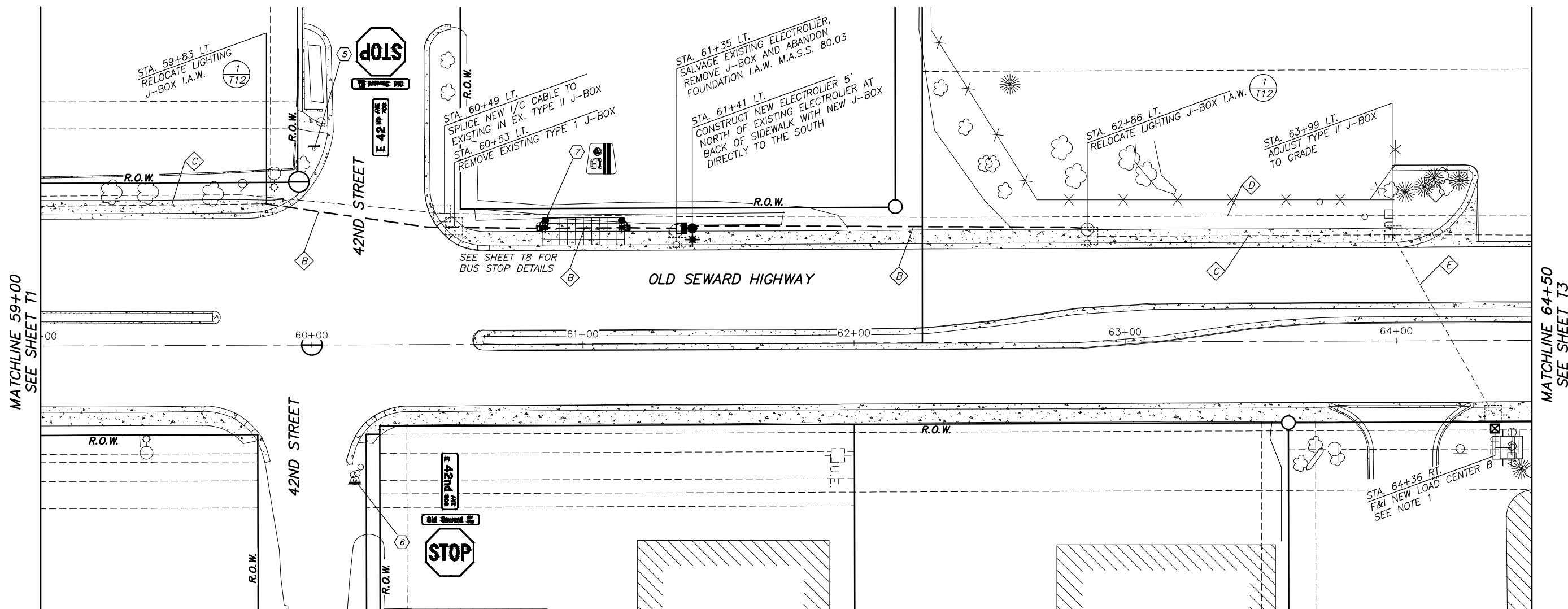
USKH
ARCHITECTURE • ENGINEERING
LAND SURVEYING • PLANNING

STATE OF ALASKA
WILLIAM A. WEBB
1/4/2013
CE 12023
REGISTERED PROFESSIONAL ENGINEER

UNIVERSITY OF ANCHORAGE

PUBLIC WORKS DEPARTMENT
10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH A.D.
SIGNING & ILLUMINATION
34TH AVENUE TO TUDOR ROAD
SCALE: 1"=20'
DATE: OCT 2012
ACCT. NO.
GRID: SW1631, SW1632, SW1731, SW1732
SHEET T1 of T14

FILE NO.



MATCHLINE 59+00
SEE SHEET T1

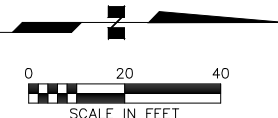
MATCHLINE 64+50
SEE SHEET T3

NOTES

- EXISTING LIGHTING SYSTEM IS POWERED FROM A UTILITY PEDESTAL AND CONTROLLED FROM PHOTOCELL ON ADJACENT UTILITY LOAD CENTER. RECONFIGURE SYSTEM TO POWER AND CONTROL LIGHTING FROM NEW LOAD CENTER.

CONDUIT NOTES

- B 2 RMC" 2-3c#8 (B1,B2)
- C EX 2": 2-3c#8 (B1,B2)
REMOVE EXISTING LIGHTING CABLES
- D EX 2": 1-25pr#19 (1/C)
REMOVE EXISTING 1/C CABLES
- E EX 2" RMC
2-3c#8 (B1,B2), 1-3c#6 (B3)
REMOVE EXISTING LIGHTING CABLES



FIELD BOOKS	BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REV	DATE	DESCRIPTION	BY
DESIGN: MOA Book 3576, Pages 1 - 73	GAAB 93	MOA 1988 Benchmark Book, D-54	113.95'	BASE	R&M		TELEPHONE	R&M					
				TOPOGRAPHY	R&M		ELECTRIC	R&M					
				PROFILE	R&M		CABLE TV	R&M					
				SANITARY SEWER	R&M		TRAFFIC SIGNAL	R&M					
				STORM SEWER	R&M		DESIGN	USKH	WW				
				WATER	R&M		QUANTITIES	USKH	WW				
				GAS	R&M		MUN. FINAL CHECK						

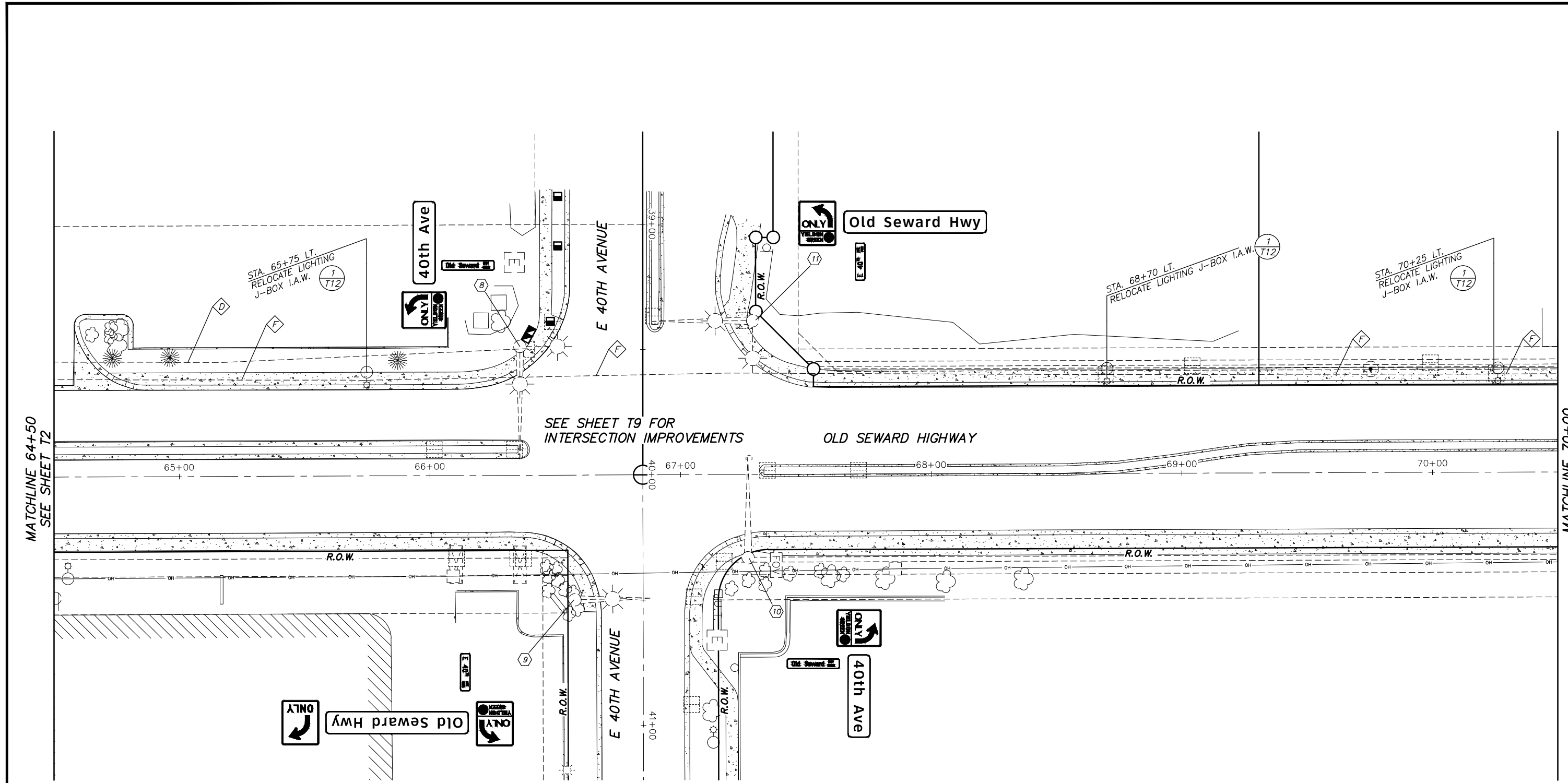
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STATE OF ALASKA
WILLIAM A. WEBB
1/4/2013
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UNIVERSITY OF ANCHORAGE

PUBLIC WORKS DEPARTMENT
10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH A.D.
SIGNING & ILLUMINATION
34TH AVENUE TO TUDOR ROAD
SCALE: 1"=20'
DATE: OCT 2012
ACCT. NO.
GRID: SW1631, SW1632, SW1731, SW1732
SHEET T2 of T14

FILE NO.



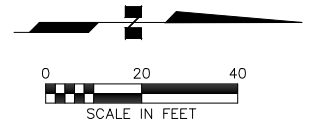
MATCHLINE 64+50
SEE SHEET T2

MATCHLINE 70+00
SEE SHEET T4

SEE SHEET T9 FOR
INTERSECTION IMPROVEMENTS

CONDUIT NOTES

- D EX. 2":1-25pr#19 (1/C)
REMOVE EXISTING 1/C CABLES
- F EX. 2" RMC
1-3c#6 (B2)
REMOVE EXISTING LIGHTING CABLES



FIELD BOOKS		BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REV	DATE	DESCRIPTION	BY
DESIGN: MOA Book 3576, Pages 1 - 73		GAAB 93	MOA 1988 Benchmark Book, D-54	113.95'	BASE	R&M		TELEPHONE	R&M					
STAKING:					TOPOGRAPHY	R&M		ELECTRIC	R&M					
ASBUILT:					PROFILE	R&M		CABLE TV	R&M					
CONTRACTOR:		BASIS OF DATUM: 1972 N.G.S. ADJUSTED DATUM			SANITARY SEWER	R&M		TRAFFIC SIGNAL	R&M					
INSPECTOR:					STORM SEWER	R&M		DESIGN	USKH	WW				
CONSTRUCTION RECORD					WATER	R&M		QUANTITIES	USKH	WW				
					GAS	R&M		MUN. FINAL CHECK						
					PLAN CHECK					REVISIONS				

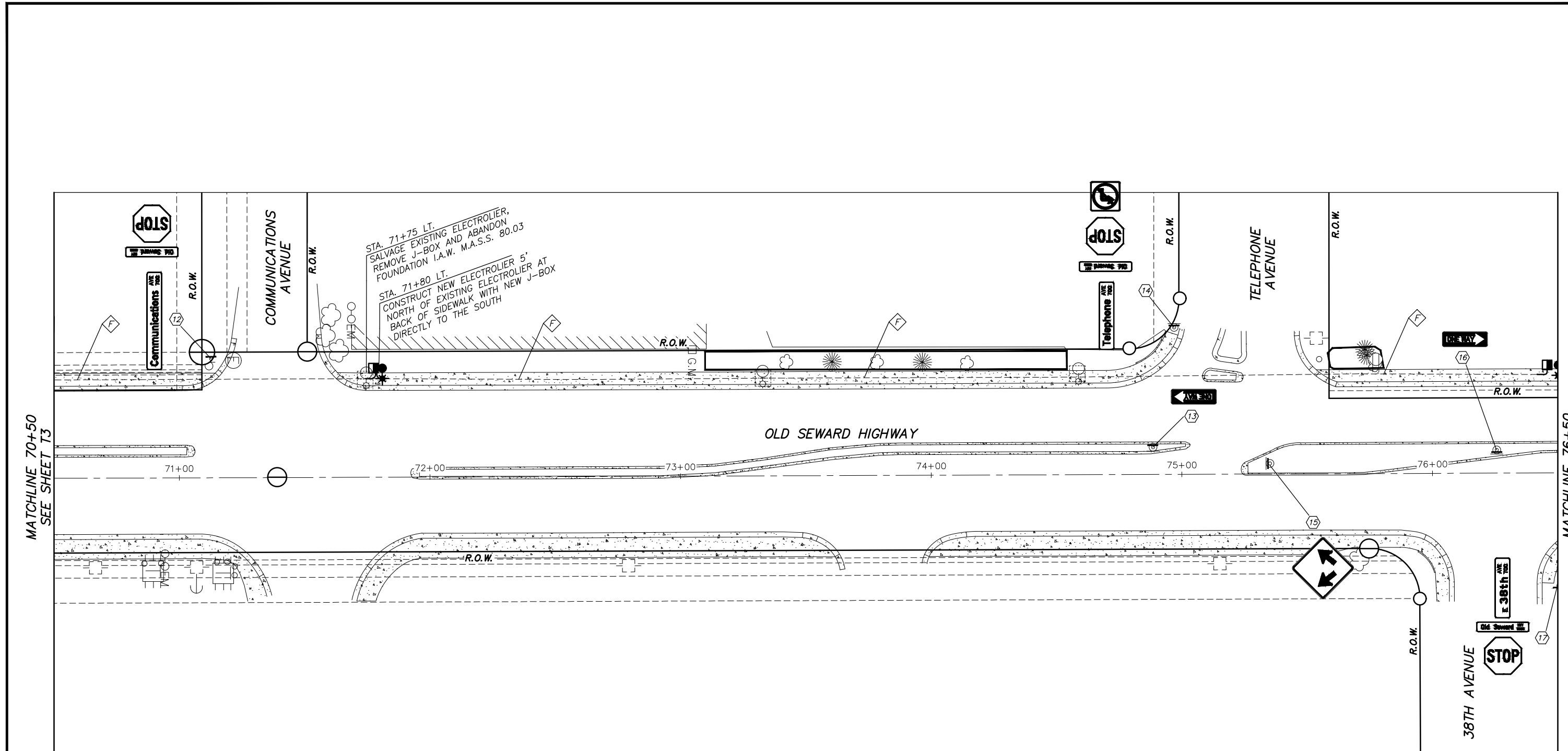
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STATE OF ALASKA
WILLIAM A. WEBB
1/4/2013
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MUNICIPALITY OF ANCHORAGE

PUBLIC WORKS DEPARTMENT
10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH A.D.
SIGNING & ILLUMINATION
34TH AVENUE TO TUDOR ROAD
SCALE: 1"=20'
DATE: OCT 2012
ACCT. NO.
GRID: SW1631, SW1632, SW1731, SW1732
SHEET **T3** of **T14**

FILE NO.

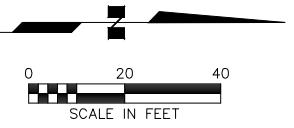


MATCHLINE 70+50
SEE SHEET T3

MATCHLINE 76+50
SEE SHEET T5

CONDUIT NOTES

EX. 2": 1-3c#6 (B2)
REMOVE EXISTING LIGHTING CABLES



FIELD BOOKS	BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REV	DATE	DESCRIPTION	BY
DESIGN: MOA Book 3576, Pages 1 - 73	GAAB 93	MOA 1988 Benchmark Book, D-54	113.95'	BASE	R&M		TELEPHONE	R&M					
STAKING:				TOPOGRAPHY	R&M		ELECTRIC	R&M					
ASBUILT:				PROFILE	R&M		CABLE TV	R&M					
CONTRACTOR:				SANITARY SEWER	R&M		TRAFFIC SIGNAL	R&M					
INSPECTOR:				STORM SEWER	R&M		DESIGN	USKH	WW				
				WATER	R&M		QUANTITIES	USKH	WW				
				GAS	R&M		MUN. FINAL CHECK						

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STATE OF ALASKA
REGISTERED PROFESSIONAL ENGINEER
WILLIAM A. WEBB
1/4/2013
OE 12023

MUNICIPALITY OF ANCHORAGE

PUBLIC WORKS DEPARTMENT
10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH A.D.
SIGNING & ILLUMINATION
34TH AVENUE TO TUDOR ROAD
DATE: OCT 2012 GRID: SW1631, SW1632, SW1731, SW1732
ACCT. NO. SHEET T4 of T14

FILE NO.

STA. 76+50 LT.
CONSTRUCT NEW ELECTROLIER 5'
SOUTH OF EXISTING ELECTROLIER AT
BACK OF SIDEWALK WITH NEW J-BOX
DIRECTLY TO THE SOUTH

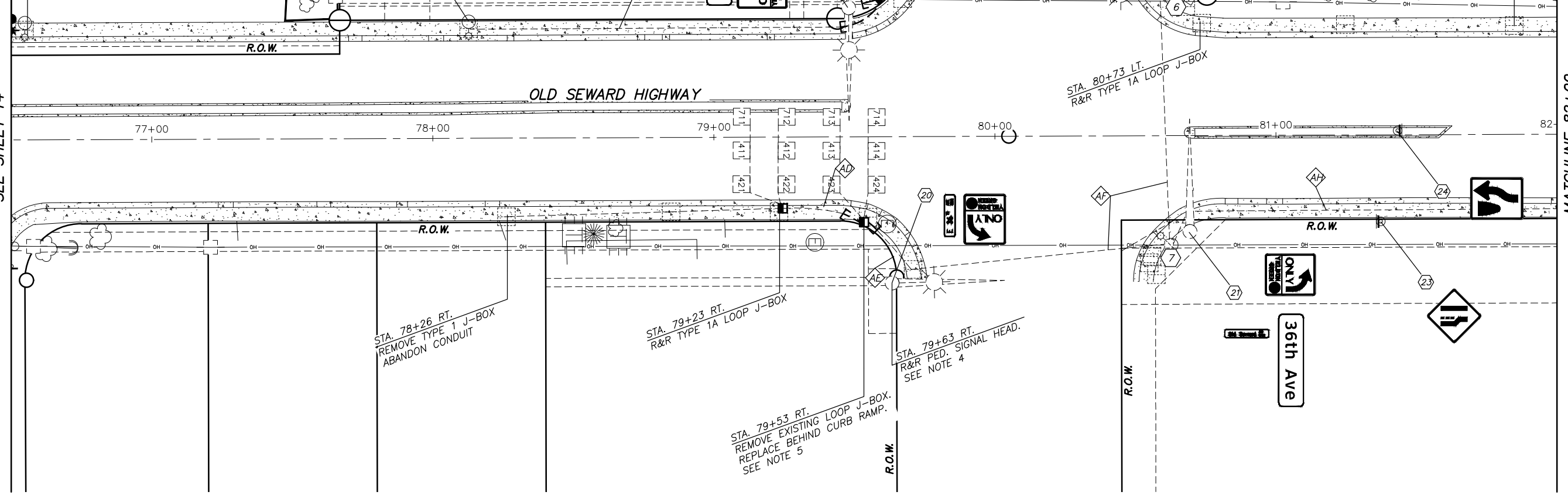
STA. 76+56 LT.
SALVAGE EXISTING ELECTROLIER,
REMOVE J-BOX AND ABANDON
FOUNDATION I.A.W. M.A.S.S. 80.03

PRESERVE EXISTING
"ADOPT A HIGHWAY"
SIGNS

STA. 79+46 LT.
REMOVE AND RELOCATE
EX. PED. BUTTONS I.A.W.
M.A.S.S. DTL 80.-28

STA. 79+60 LT.
REMOVE EXISTING TYPE II J-BOX
AND REPLACE WITH NEW TYPE III
J-BOX BEHIND NEW CURB RAMP

STA. 81+85 LT.
ADJUST J-BOX TO
GRADE



MATCHLINE 76+50
SEE SHEET T4

MATCHLINE 82+00
SEE SHEET T6

CONDUIT NOTES

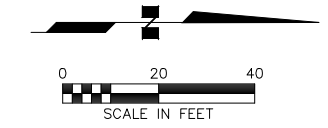
- AA EX. 2": 1-3c#8
EX. 3": 4-7c#14, 2-5c#14, 2-3c#14,
1-3c#14(CL), 1-3c#20(OPC)
- AB EX. 2": 3-7pr#18
EX. 2": 1-25pr#19
EX. 2": 1-3c#8
- AC EX. 3": 4-7c#14, 2-5c#14, 2-3c#14,
1-3c#14(CL), 1-3c#20(OPC), 3-7pr#18
EX. 2": SPARE
EX. 2": 1-3c#8
EX. 2": 2-3c#8
EX. 3": 1-25pr#19

CONDUIT NOTES

- AD EX. 2": 1-7pr#18
- AE EX. 2": 2-7pr#18
- AF EX. CONDUIT: 2-7pr#18
REMOVE NB DETECTOR LEAD-INS
PRESERVE OTHER EXISTING CABLES
- AG EX. 2": 1-3c#8
- AP EX. 2": 1-3c#8
REMOVE EXISTING LIGHTING CABLES

CONSTRUCTION NOTES:

1. SPLICE EXISTING LOOPS TO NEW 7pr#18 CABLES. COORDINATE WITH MOA SIGNAL ELECTRONICS FOR LOOP COLOR CODE ASSIGNMENTS.
2. REMOVE EXISTING CABLES AND SPLICE NEW TO EXISTING AS FOLLOWS:
1-3c#8: IN LIGHT POLE BASE ~ 160' WEST OF INTERSECTION
1-25pr: IN TYPE II JUNCTION BOX ~ 200' WEST OF INTERSECTION
3-7pr: ONE EXTENDS TO JUNCTION BOX 20' WEST, ONE TO JUNCTION BOX 40' WEST, AND ONE TO JUNCTION BOXES 180' AND 270' WEST OF INTERSECTION.
3. REMOVE CABLES FOR SOUTHWEST SIGNAL POLE, EASTBOUND AND NORTHBOUND DETECTION, AND INTERCONNECT TO THE WEST. PULL NEW CABLES THROUGH EXISTING CONDUIT TO EXISTING TRAFFIC SIGNAL CONTROLLER.
4. SALVAGE PEDESTRIAN HEAD FROM EAST SIDE OF POLE 3. F&I NEW PEDESTRIAN SIGNAL HEAD ON WEST SIDE OF POLE, FACING NORTH.
5. EXTEND LOOP DETECTOR CONDUIT (PVC) TO NEW JUNCTION BOX LOCATION. PULL NEW LOOP WIRE THROUGH LOOP CONDUITS VIA ACCESS AT CONDUIT X-BODIES. TYPICAL OF 6.
6. EXTEND NEW LIGHTING CONDUCTORS TO EXISTING LOAD CENTER.
7. SPLICE NEW LIGHTING CONDUCTORS TO EXISTING.



FIELD BOOKS	BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REV	DATE	DESCRIPTION	BY
DESIGN: MOA Book 3576, Pages 1 - 73	GAAB 93	MOA 1988 Benchmark Book, D-54	113.95'	BASE	R&M		TELEPHONE	R&M					
STAKING:				TOPOGRAPHY	R&M		ELECTRIC	R&M					
ASBUILT:				PROFILE	R&M		CABLE TV	R&M					
CONTRACTOR:				SANITARY SEWER	R&M		TRAFFIC SIGNAL	R&M					
INSPECTOR:				STORM SEWER	R&M		DESIGN	USKH	WW				
				WATER	R&M		QUANTITIES	USKH	WW				
				GAS	R&M		MUN. FINAL CHECK						

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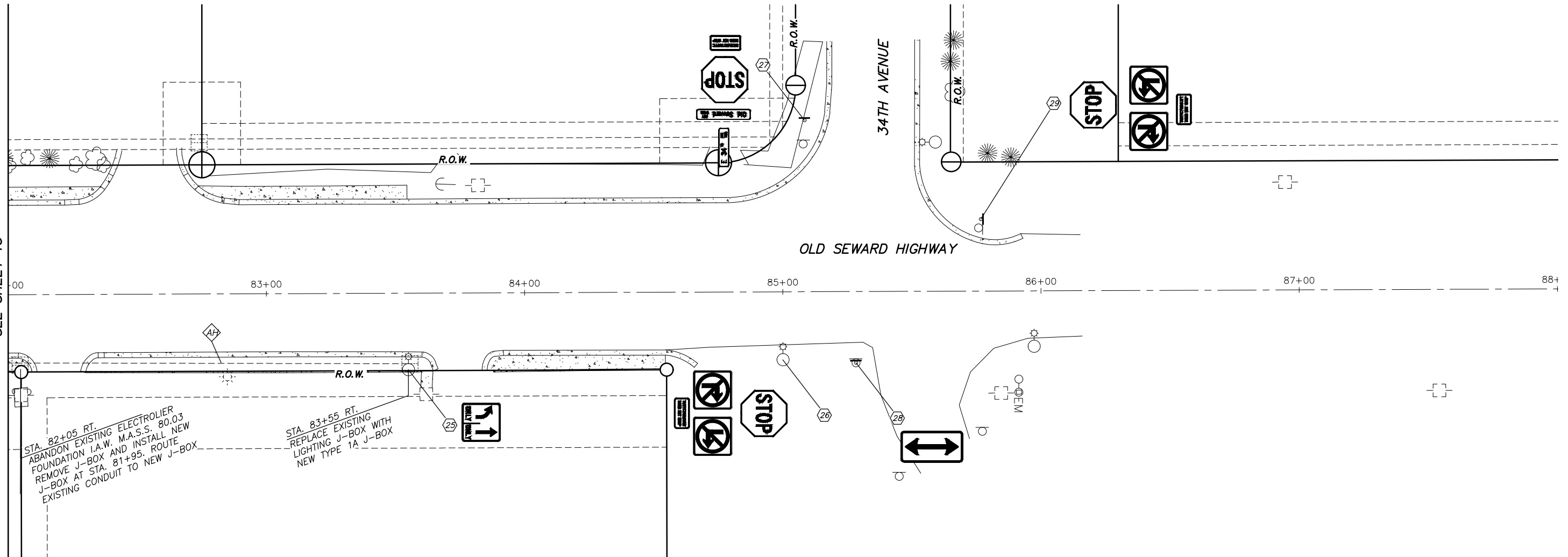
STATE OF ALASKA
WILLIAM A. WEBB
1/4/2013
CE 12023
REGISTERED PROFESSIONAL ENGINEER

MUNICIPALITY OF ANCHORAGE

PUBLIC WORKS DEPARTMENT
10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH A.D
SIGNING & ILLUMINATION
34TH AVENUE TO TUDOR ROAD
SCALE: 1"=20'
DATE: OCT 2012
GRID: SW1631, SW1632, SW1731, SW1732
ACCT. NO.
SHEET T5 of T14

FILE NO.

MATCHLINE 82+00
SEE SHEET T5

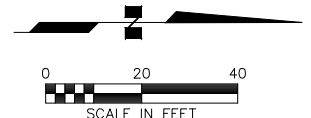


STA. 82+05 RT.
ABANDON EXISTING ELECTROLIER
FOUNDATION I.A.W. M.A.S.S. 80.03
REMOVE J-BOX AND INSTALL NEW
J-BOX AT STA. 81+95. ROUTE
EXISTING CONDUIT TO NEW J-BOX

STA. 83+55 RT.
REPLACE EXISTING
LIGHTING J-BOX WITH
NEW TYPE 1A J-BOX

CONDUIT NOTES:

⬠ EX 2": 1-3c#8
REMOVE EXISTING LIGHTING CABLES



FIELD BOOKS	BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REV	DATE	DESCRIPTION	BY
DESIGN: MOA Book 3576, Pages 1 - 73	GAAB 93	MOA 1988 Benchmark Book, D-54	113.95'	BASE	R&M		TELEPHONE	R&M					
STAKING:				TOPOGRAPHY	R&M		ELECTRIC	R&M					
ASBUILT:				PROFILE	R&M		CABLE TV	R&M					
CONTRACTOR:				SANITARY SEWER	R&M		TRAFFIC SIGNAL	R&M					
INSPECTOR:				STORM SEWER	R&M		DESIGN	USKH	WW				
CONSTRUCTION RECORD				WATER	R&M		QUANTITIES	USKH	WW				
				GAS	R&M		MUN. FINAL CHECK						

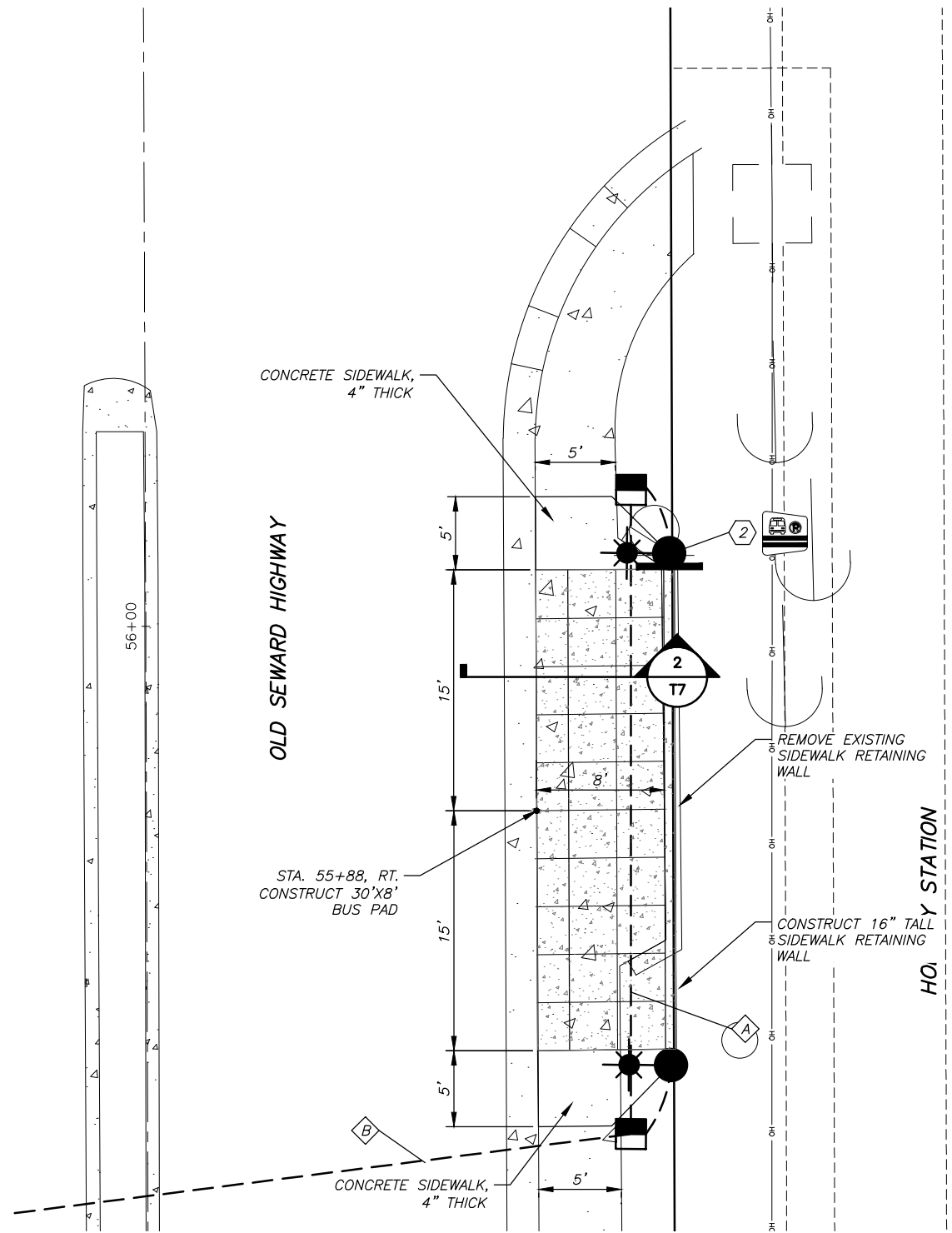
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10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH A.D.
SIGNING & ILLUMINATION
34TH AVENUE TO TUDOR ROAD
SCALE: 1"=20'
DATE: OCT 2012
ACCT. NO.
GRID: SW1631, SW1632, SW1731, SW1732
SHEET **T6** of **T14**

FILE NO.



1 HOLIDAY BUS STOP PLAN
SCALE: 1"=5'

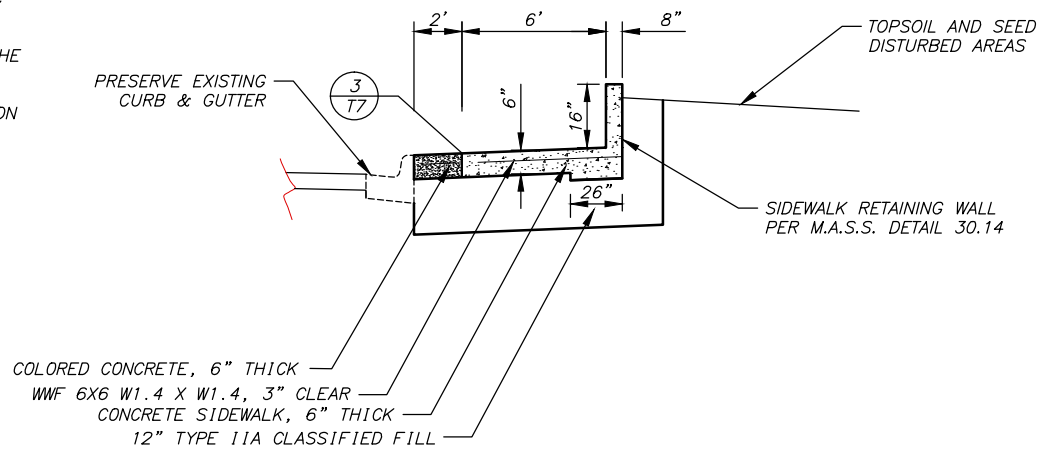
NOTES:

- DURING BUS STOP CONSTRUCTION, MOUNT THE BUS STOP SIGN AT A TEMPORARY BUS STOP LOCATION IDENTIFIED BY THE PUBLIC TRANSPORTATION DEPARTMENT.
- INSTALL BUS STOP ELECTROLIERS SO FOUNDATIONS ARE AT THE ENDS OF THE SIDEWALK RETAINING WALL.
- ENDS OF SIDEWALK RETAINING WALL SHALL BE SAME ELEVATION AS TOP OF ADJACENT BUS STOP ELECTROLIER FOUNDATION. TRANSITION TO FULL WALL HEIGHT WITH 2:1 SLOPE.
- PROVIDE CONTRACTION JOINTS ON 3'x3' GRID.
- FORM CONSTRUCTION JOINTS AROUND BUS STOP ELECTROLIER FOUNDATIONS.

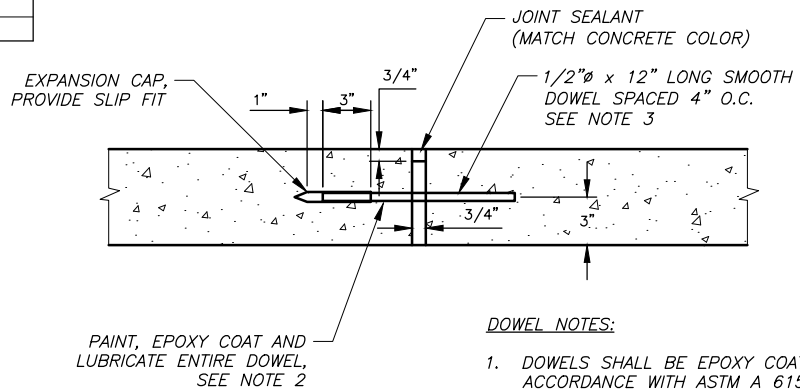
CONDUIT NOTES:

- A 2" RMC: 1-3c#8 (B1)
- B 2" RMC: 2-3c#8 (B1,B2)

STRUCTURE SUMMARY			
STATION	OFFSET	TYPE	REMARKS
55+68.1	30.2 RT	1A J-BOX	
55+72.5	32.7 RT	BUS STOP ELECTROLIER	
56+04.5	32.7 RT	BUS STOP ELECTROLIER	
56+08.4	30.2 RT	1A J-BOX	



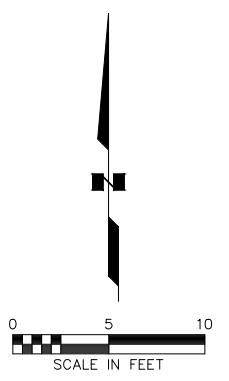
2 HOLIDAY BUS STOP SECTION
SCALE: N.T.S



DOWEL NOTES:

- DOWELS SHALL BE EPOXY COATED STEEL IN ACCORDANCE WITH ASTM A 615M, GRADE 280 OR 420.
- DOWEL BARS SHALL BE PAINTED AND LUBRICATED WITH BOND BREAKER OVER THE ENTIRE BAR PRIOR TO PLACEMENT. LUBRICANT SHALL BE PETROLEUM PARAFFIN BASED.
- ALL WORK RELATED TO FURNISHING AND INSTALLING DOWEL BARS SHALL BE CONSIDERED INCIDENTAL TO CONCRETE PAY ITEMS AND NO SEPARATE PAYMENT SHALL BE MADE.

3 DOWEL DETAIL
SCALE: N.T.S



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STAKING:				TOPOGRAPHY	R&M		ELECTRIC	R&M					
ASBUILT:				PROFILE	R&M		CABLE TV	R&M					
CONTRACTOR:				SANITARY SEWER	R&M		TRAFFIC SIGNAL	R&M					
INSPECTOR:				STORM SEWER	R&M		DESIGN	USKH	WW				
				WATER	R&M		QUANTITIES	USKH	WW				
				GAS	R&M		MUN. FINAL CHECK						

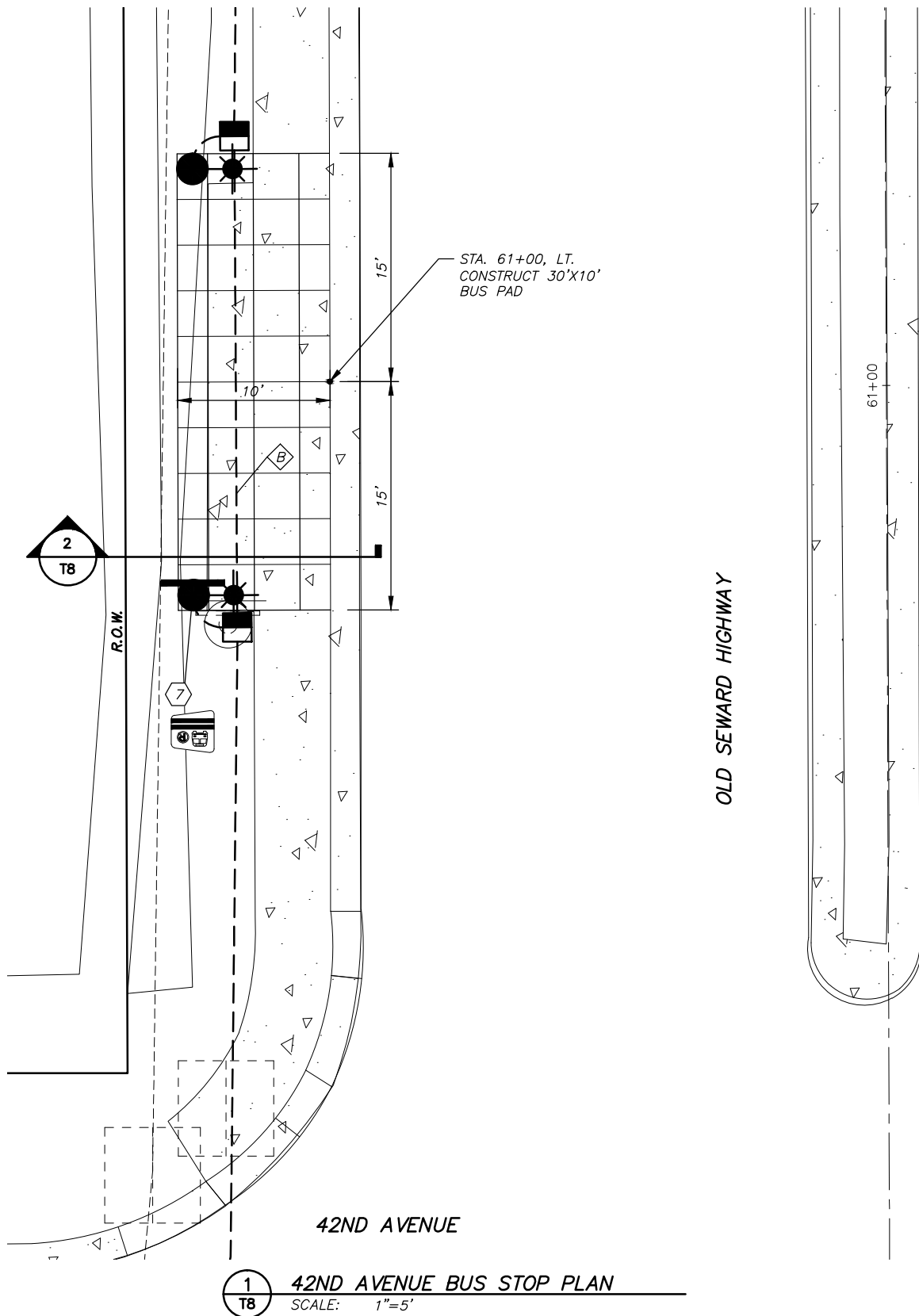
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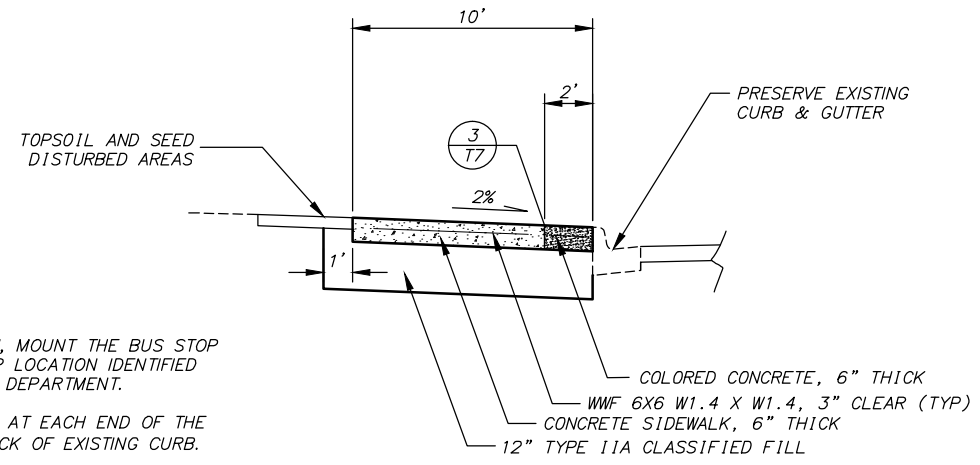
MUNICIPALITY OF ANCHORAGE

PUBLIC WORKS DEPARTMENT
10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH C.D
HOLIDAY BUS STOP
34TH AVENUE TO TUDOR ROAD
SCALE: VARIES DATE: OCT 2012 GRID: SW1631, SW1632, SW1731, SW1732 SHEET **T7** of **T14**

FILE NO.



1
T8 **42ND AVENUE BUS STOP PLAN**
SCALE: 1"=5'



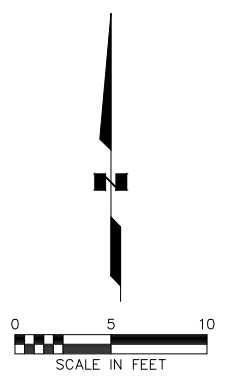
2
T8 **42ND AVENUE BUS STOP SECTION**
SCALE: N.T.S

- NOTES:**
1. DURING BUS STOP CONSTRUCTION, MOUNT THE BUS STOP SIGN AT A TEMPORARY BUS STOP LOCATION IDENTIFIED BY THE PUBLIC TRANSPORTATION DEPARTMENT.
 2. INSTALL BUS STOP ELECTROLIERS AT EACH END OF THE BUS STOP PAD, 9' FROM THE BACK OF EXISTING CURB.
 3. FORM CONSTRUCTION JOINTS AROUND BUS STOP ELECTROLIER FOUNDATIONS.
 4. PROVIDE CONTRACTION JOINTS ON 3'x3' GRID.

CONDUIT NOTES

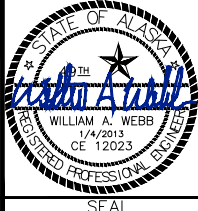
◇ 2" RMC: 2-3c#8 (B1,B2)

STRUCTURE SUMMARY			
STATION	OFFSET	TYPE	REMARKS
60+84.3	42.5 LT	1A J-BOX	
60+86.4	45.5 LT	BUS STOP ELECTROLIER	
61+14.4	45.5 LT	BUS STOP ELECTROLIER	
61+16.5	42.5 LT	1A J-BOX	



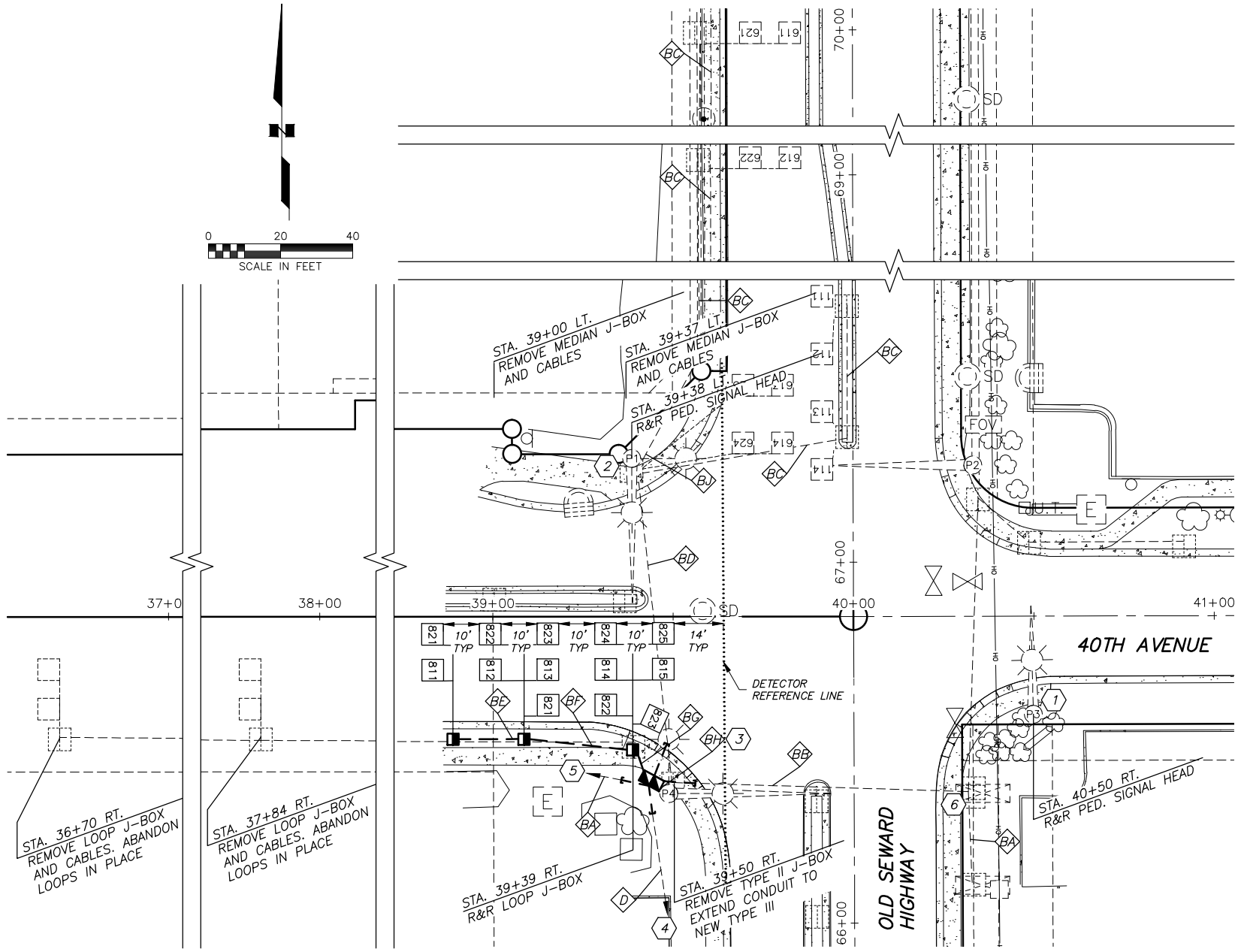
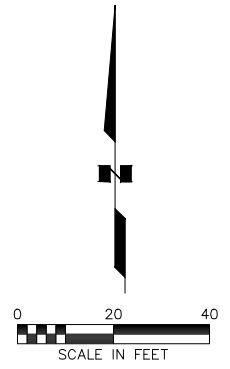
FIELD BOOKS	BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REV	DATE	DESCRIPTION	BY
DESIGN: MOA Book 3576, Pages 1 - 73	GAAB 93	MOA 1988 Benchmark Book, D-54	113.95'	BASE	R&M		TELEPHONE	R&M					
STAKING:				TOPOGRAPHY	R&M		ELECTRIC	R&M					
ASBUILT:				PROFILE	R&M		CABLE TV	R&M					
CONTRACTOR:				SANITARY SEWER	R&M		TRAFFIC SIGNAL	R&M					
INSPECTOR:				STORM SEWER	R&M		DESIGN	USKH	WW				
				WATER	R&M		QUANTITIES	USKH	WW				
				GAS	R&M		MUN. FINAL CHECK						

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PUBLIC WORKS DEPARTMENT
10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH C.D
42ND AVE BUS STOP
34TH AVENUE TO TUDOR ROAD
SCALE: VARIES DATE: OCT 2012 GRID: SW1631, SW1632 ACCT. NO. SHEET **T8** of **T14**

FILE NO.



STRUCTURE SUMMARY			
STATION	OFFSET	TYPE	REMARKS
38+89.0	34.0 RT	1A J-BOX	
39+09.0	34.0 RT	1A J-BOX	
39+38.7	36.6 RT	1A J-BOX	R&R EX. J-BOX
39+44.0	45.5 RT	III J-BOX	

CONSTRUCTION NOTES:

- 1 SALVAGE PED SIGNAL FROM EAST SIDE OF SE SIGNAL POLE. F&I NEW PED SIGNAL ON WEST SIDE OF SE SIGNAL POLE, FACING NORTH.
- 2 SALVAGE PED SIGNAL FROM WEST SIDE OF NW SIGNAL POLE. F&I NEW PED SIGNAL ON EAST SIDE OF NW SIGNAL POLE, FACING SOUTH. IN ADDITION, REMOVE AND RELOCATE THE EXISTING PEDESTRIAN PUSH BUTTONS TO HEIGHT SHOWN IN M.A.S.S. DTL 80-28.
- 3 CONDUIT SIZES AND QUANTITIES WERE DETERMINED FROM PREVIOUS PROJECT DRAWINGS. EXTEND ALL CONDUIT FROM REMOVED TYPE II J-BOX TO NEW TYPE III J-BOX AND REDLINE ANY CONDUIT NOT SHOWN ON THE PLANS.
- 4 INSTALL A SINGLE CONTINUOUS 1-25pr#19 BETWEEN 40TH AVE/ OLD SEWARD HWY CONTROLLER AND TYPE II J-BOX AT 42ND AVE/OLD SEWARD HWY. SEE SHEET T2.
- 5 INSTALL 1-3c#8 BETWEEN INTERSECTION LOAD CENTER AND FIRST 40TH AVE ELECTROLIER TO THE WEST (APPROX. 275' FROM INTERSECTION).
- 6 REMOVE CABLES FOR SOUTHWEST SIGNAL POLE; NORTHWEST SIGNAL POLE; EASTBOUND, SOUTHBOUND, AND SOUTHBOUND LEFT TURN DETECTION; AND INTERCONNECT TO THE SOUTH. PULL NEW CABLES THROUGH EXISTING/EXTENDED CONDUIT TO EXISTING TRAFFIC SIGNAL CONTROLLER. TERMINATE NEW CABLES IN EXISTING SIGNAL EQUIPMENT AND SPLICE EXISTING LOOPS TO NEW CABLES.

NOTES:

1. WORK AT THIS INTERSECTION GENERALLY CONSISTS OF THE FOLLOWING:
 - REMOVE AND REPLACE CABLE TO NW AND SW SIGNAL POLES AND SOUTHBOUND AND EASTBOUND DETECTION
 - REPLACE TYPE II J-BOX IN SW CORNER WITH TYPE III J-BOX
 - ABANDON ADVANCE EASTBOUND DETECTION AND REPLACE EASTBOUND STOP BAR DETECTION AND J-BOXES
 - RELOCATING TWO PED. SIGNAL HEADS AND TWO PED. PUSH BUTTONS.
2. SURVEY HAS NOT BEEN COMPLETED FOR THE ENTIRE LOOP DETECTOR WORK AREA. LOCATIONS OF PROPOSED LOOP DETECTORS ARE SHOWN FROM A REFERENCE LINE, WHICH EXTENDS FROM FACE OF CURB AS SHOWN.
3. REMOVE EXISTING EASTBOUND STOPBAR LOOP DETECTORS (311-314, 813-814, & 823-824). THE LOCATIONS OF THE AFOREMENTIONED LOOP DETECTORS ARE NOT SHOWN ON PLAN, BUT SHOULD ROUGHLY MATCH NEW LOOP DETECTOR LOCATIONS. REMOVAL OF EXISTING LOOP DETECTORS IS INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
4. INSTALL LOOP DETECTORS (311-315, 811-815, & 821-823) I.A.W. M.A.S.S. STD. DTLs. 80-52, 80-53, & 80-54, AND AS DIRECTED BY THE ENGINEER. WITH THE EXCEPTION OF LOOP 823, CENTER NEW LOOP DETECTORS WITHIN THE TRAVEL LANES.
5. REMOVAL AND REPLACEMENT OF CURB & GUTTER AND SIDEWALK AS REQUIRED FOR LOOP DETECTOR HOME RUN AND JUNCTION BOX INSTALLATION IS INCIDENTAL PURSUANT TO M.A.S.S. SECTION 80.03 REMOVING AND REPLACING IMPROVEMENTS
6. COORDINATE WORK WITH MOA SIGNAL ELECTRONICS SHOP (343-8355) PRIOR TO THE REMOVAL OF EXISTING DETECTOR LOOPS OR REMOVAL OF EXISTING LOOP LEAD-IN CABLES.
7. SALVAGE PED SIGNAL FROM EAST SIDE OF SE SIGNAL POLE. F&I NEW PED SIGNAL ON WEST SIDE OF SE SIGNAL POLE, FACING NORTH.
8. SALVAGE PED SIGNAL FROM WEST SIDE OF NW SIGNAL POLE. F&I NEW PED SIGNAL ON EAST SIDE OF NW SIGNAL POLE, FACING SOUTH.

CONDUIT NOTES

- ◇ D EX 2": 1-25pr#19 I/C
REMOVE EXISTING I/C CABLE
- ◇ BA EX 2": 1-3c#8 (40TH LTG)
REMOVE EXISTING 40TH LTG CABLE
- ◇ BB REMOVE EXISTING CABLES
EX 3": 4-7c#14, 2-5c#14, 2-3c#14, 1-3c#14 (CL), 1-3c#20 (OPC), 3-7pr#18
EX 3": 3-7c#14, 2-5c#14, 2-3c#14, 1-3c#14(CL), 1-3c#20 (OPC), 3-7pr#18
EX 3": SPARE
EX 2": 1-25pr#19 I/C
EX 2" 1-3c#8 (INT. LTG)
EX 2": 1-3c#8 (40TH LTG)
- ◇ BC REMOVE EXISTING CABLES
EX 2": 1-7pr#18

- ◇ BD REMOVE EXISTING CABLES
EX 3": 3-7c#14, 2-5c#14, 2-3c#14, 1-3c#14(CL), 1-3c#20 (OPC), 3-7pr#18
EX 2": 1-3c#8 (INT. LTG)
EX 2": SPARE
- ◇ BE 2" RMC: 1-7pr#18
- ◇ BF 2" RMC: 2-7pr#18
- ◇ BG 2" RMC: 3-7pr#18
- ◇ BH REMOVE EXISTING CABLES
EX 3": 4-7c#14, 2-5c#14, 2-3c#14, 1-3c#14(CL), 1-3c#20(OPC)
EX 2": 2-3c#8 (INT. LTG)
EX 2": SPARE
- ◇ BI REMOVE EXISTING CABLES
EX 3": 3-7c#14, 2-5c#14, 2-3c#14, 1-3c#14(CL), 1-3c#20 (OPC),
EX 2": 1-3c#8 (INT. LTG)
EX 2": SPARE

FIELD BOOKS	BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REV	DATE	BY	REV	DATE	DESCRIPTION	BY
DESIGN: MOA Book 3576, Pages 1 - 73	GAAB 93	MOA 1988 Benchmark Book, D-54	113.95'	BASE	R&M		TELEPHONE	R&M								
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ASBUILT:				PROFILE	R&M		CABLE TV	R&M								
CONTRACTOR:				SANITARY SEWER	R&M		TRAFFIC SIGNAL	R&M								
INSPECTOR:				STORM SEWER	R&M		DESIGN	USKH	WW							
CONSTRUCTION RECORD				WATER	R&M		QUANTITIES	USKH	WW							
				GAS	R&M		MUN. FINAL CHECK									

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UNIVERSITY OF ANCHORAGE

PUBLIC WORKS DEPARTMENT
10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH C.D
40TH AVENUE SIGNAL IMPROVEMENTS
34TH AVENUE TO TUDOR ROAD
SCALE: 1"=20'
DATE: OCT 2012
ACCT. NO.
SHEET T9 of T14

FILE NO.

POST NO.	STA	OFFSET (FT)	TYPE	LEGEND	SIZE		AREA SQ FT	SIGN FACES	POSTS NO, SIZE, TYPE	THICKNESS (IN)		REMARKS
					INCHES					BRACED?		
					W	H				Y	N	
1	55+13	56.5 LT	R3-7R	RIGHT LANE MUST TURN RIGHT	36	36	9.0	N	1-2.5" PT	0.125		
2	56+04	32.7 RT	R7-107M	BUS STOP	18	18	2.0	S		0.125		MOUNT ON NEW BUS STOP ELECTROLIER
3	56+80	32.5 RT	R2-1	35 MPH	30	36	7.5	S	1-2.5" PT	0.125		
4	58+84	56.6 LT	R3-7R	RIGHT LANE MUST TURN RIGHT	36	36	9.0	N	1-2.5" PT	0.125		
5	60+01	72.4 LT	R1-1	STOP	36	36	9.0	W	1-2.5" PT	0.125		
			D3-1D	E. 42 ND STREET	42	12	7.0	W		0.125		
			D3-1D	OLD SEWARD HIGHWAY	42	8	4.7	W		0.125		
6	60+16	50.0 RT	R1-1	STOP	36	36	9.0	E	1-2.5" PT	0.125		
			D3-1D	E. 42 ND STREET	42	12	7.0	E		0.125		
			D3-1D	OLD SEWARD HIGHWAY	42	8	4.7	E		0.125		
7	60+86	45.6 LT	R7-107M	BUS STOP	18	18	2.0	S		0.125		MOUNT ON NEW BUS STOP ELECTROLIER
8	66+36	51.4 LT	D3-1B	40TH AVE	66	18	8.3	N		0.125		MOUNT ON EXISTING SIGNAL POLE
			R10-100	LEFT TURN YIELD ON GREEN	30	36	7.5	N		0.125		
			D3-1D	OLD SEWARD HIGHWAY	42	8	4.7	E/W		0.125		
9	66+58	49.8 RT	R3-5R	RIGHT TURN ONLY	30	36	9.0	W		0.125		MOUNT ON EXISTING SIGNAL POLE
			D3-1B	OLD SEWARD HIGHWAY	114	18	14.3	W		0.125		
			R10-100	LEFT TURN YIELD ON GREEN	30	36	7.5	W		0.125		
			D3-1D	E 40TH AVE	30	8	3.3	N/S		0.125		
10	67+27	33.2 RT	D3-1B	40TH AVE	66	18	8.25	S		0.125		MOUNT ON EXISTING SIGNAL POLE
			R10-100	LEFT TURN YIELD ON GREEN	30	36	7.5	S		0.125		
			D3-1D	OLD SEWARD HIGHWAY	42	8	4.7	E/W		0.125		
11	67+29	61.4 LT	D3-1B	OLD SEWARD HIGHWAY	114	18	14.3	E		0.125		MOUNT ON EXISTING SIGNAL POLE
			R10-100	LEFT TURN YIELD ON GREEN	30	36	7.5	E		0.125		
			D3-1D	E 40TH AVE	30	8	3.3	N/S		0.125		
12	71+13	47.5 LT	R1-1	STOP	36	36	9.0	W	1-2.5" PT	0.125		
			D3-1D	COMMUNICATIONS AVE	78	12	13.0	N/S		0.125		
			D3-1D	OLD SEWARD HIGHWAY	42	8	4.7	E/W		0.125		
13	74+89	10.8 LT	R6-1R	ONE WAY	36	12	3.0	W		0.125		
14	74+97	58.5 LT	R1-1	STOP	36	36	9.0	W	1-2.5" PT	0.125		
			D3-1D	TELEPHONE AVE	54	12	9.0	N/S		0.125		
			D3-1D	OLD SEWARD HIGHWAY	42	8	4.7	E/W		0.125		
			R3-2	NO LEFT TURN	30	30	6.3	W		0.125		
15	75+35	4.2 RT	W12-1	LANE SPLITS	36	36	9.0	S	1-2.5" PT	0.125		
16	75+26	9.2 LT	R6-1R	ONE WAY	36	12	3.0	W	1-2" PT	0.125		
					SIGN AREA SUBTOTAL		241.6					

GENERAL SIGNING NOTES:

- EXISTING SIGNS AND HARDWARE SHALL BE REMOVED AND DELIVERED UNDAMAGED TO THE MOA PAINT AND SIGN SHOP, 2839 MT. VIEW DRIVE. COORDINATE DELIVERY WITH MOA SIGN SHOP FOREMAN.
- SIGN LOCATIONS AND OFFSETS ARE APPROXIMATE. SIGNS SHALL BE INSTALLED PER MASS STANDARD DETAILS OR AS DIRECTED BY THE ENGINEER.
- THE POST TYPES INDICATED IN THIS SUMMARY ARE ABBREVIATED:
T = SQUARE STEEL TUBE
PT = PERFORATED STEEL TUBE (0.105" WALL THICKNESS)
- D3-1B AND D3-1D SIGNS SHALL USE UPPER CASE/LOWER CASE LETTERS (I.E. Old Seward) FOR STREET NAMES.

SIGN SALVAGE SUMMARY			
STA	OFFSET	NUMBER OF SIGNS	REMARKS
55+13	LT	1	R3-7R
56+04	RT	1	R7-107M
56+80	RT	1	R2-1
58+84	LT	1	R3-7R
60+01	LT	1	R1-1
60+16	RT	2	R1-1, D3-1D
60+86	LT	1	R7-107M
66+58	RT	2	D3-1B, R10-3D
66+36	LT	3	D3-1B, D3-1D, R10-1D
67+27	RT	2	D3-1B, R10-1D
67+29	LT	3	D3-1B, D3-1D, R10-1D
71+13	LT	3	R1-1, D3-1D, D3-1D
71+84	RT	1	R1-1
74+00	RT	1	R1-1
74+97	LT	3	R1-1, D3-1D, D3-1D
79+50	RT	3	R1-1, D3-1D, D3-1D
78+13	LT	1	R2-1
79+48	LT	2	D3-1D, R10-1D
79+63	RT	1	R10-1D
80+69	RT	2	R9-5, R10-1D
80+60	LT	3	D3-1B, BIKE (SYMBOL), R10-1D
81+37	RT	1	W4-2
81+43	CL	3	R4-8, OM-1, OM-1
83+55	RT	1	R3-BL/S
85+00	RT	3	W4-4P, R1-1, R3-1
85+09	LT	3	R1-1, D3-1D, D3-1D
82+28	RT	1	W1-7
85+77	LT	3	R3-1, R1-1, R3-2

FIELD BOOKS	BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REV	DATE	DESCRIPTION	BY
DESIGN: MOA Book 3576, Pages 1 - 73	GAAB 93	MOA 1988 Benchmark Book, D-54	113.95'	BASE	R&M		TELEPHONE	R&M					
STAKING:				TOPOGRAPHY	R&M		ELECTRIC	R&M					
ASBUILT:				PROFILE	R&M		CABLE TV	R&M					
CONTRACTOR:				SANITARY SEWER	R&M		TRAFFIC SIGNAL	R&M					
INSPECTOR:				STORM SEWER	R&M		DESIGN	USKH	WW				
CONSTRUCTION RECORD				WATER	R&M		QUANTITIES	USKH	WW				
				GAS	R&M		MUN. FINAL CHECK						

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LAND SURVEYING • PLANNING

STATE OF ALASKA
WILLIAM A. WEBB
1/4/2013
CE 12023
REGISTERED PROFESSIONAL ENGINEER

MUNICIPALITY OF ANCHORAGE

PUBLIC WORKS DEPARTMENT
10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH A
SIGN SUMMARY
34TH AVENUE TO TUDOR ROAD
SCALE: N.T.S. DATE: OCT 2012 GRID: SW1631, SW1632, SW1731, SW1732 SHEET T10 of T14
ACCT. NO.

FILE NO.

SIGN SUMMARY TABLE												
POST NO.	STA	OFFSET	TYPE	LEGEND	SIZE		AREA SQ FT	SIGN FACES	POSTS NO, SIZE, TYPE	THICKNESS (IN)		REMARKS
					INCHES					BRACED?		
					W	H				Y	N	
17	76+50	44.9 RT	R1-1	STOP	36	36	9.0	E	1-2.5" PT	0.125		
			D3-1D	38TH AVE	48	12	8.0	E			0.125	
			D3-1D	OLD SEWARD HIGHWAY	42	8	4.7	E			0.125	
18	78+13	41.4 LT	R2-1	35 MPH	30	36	7.5	N		0.125		MOUNT ON EXISTING ELECTROLIER ABOVE EXISTING ADOPT A HIGHWAY SIGNS
19	79+48	45.9 LT	D3-1B	36TH AVE	66	18	8.25	N		0.125		MOUNT ON EXISTING SIGNAL POLE
			R10-100	LEFT TURN YIELD ON GREEN	30	36	7.5	N		0.125		
			D3-1D	OLD SEWARD HIGHWAY	42	8	4.7	E/W		0.125		
20	79+63	52.0 RT	R10-100	LEFT TURN YIELD ON GREEN	30	36	7.5	W		0.125		MOUNT ON EXISTING SIGNAL POLE
			D3-1D	E 36TH AVE	30	8	3.3	N/S		0.125		
21	80+69	34.1 RT	D3-1B	36TH AVE	66	18	8.25	S		0.125		MOUNT ON EXISTING SIGNAL POLE
			R10-100	LEFT TURN YIELD ON GREEN	30	36	7.5	S		0.125		
			D3-1D	OLD SEWARD HIGHWAY	42	8	4.7	E/W		0.125		
22	80+60	50.5 LT	D3-1B	OLD SEWARD HIGHWAY	114	18	14.3	E		0.125		MOUNT ON EXISTING SIGNAL POLE
			R10-100	LEFT TURN YIELD ON GREEN	30	36	7.5	E		0.125		
			D3-1D	E 36TH AVE	30	8	3.3	N/S		0.125		
			R9-5	BIKES USE PED SIGNAL	12	18	1.5	W		0.125		
23	81+37	30.8 RT	W4-2R	RIGHT LANE ENDS	36	36	9.0	S	1-2.5" PT	0.125		
24	81+43	1.43 LT	R4-7	KEEP RIGHT SYMBOL	24	30	5.0	N	1-2" PT		0.125	
25	83+55	29.7 RT	R3-8L/S	LEFT TURN ONLY, STRAIGHT ONLY	36	30	7.5	S		0.125		MOUNT ON EXISTING ELECTROLIER
26	85+00	26.4 RT	R1-1	STOP	36	36	9.0	S	1-2.5" PT	0.125	0.125	
			R3-1	NO RIGHT TURN	30	30	6.3	S			0.125	
			R3-2	NO LEFT TURN	30	30	6.3	N			0.125	
			W4-4A	TRAFFIC FROM RIGHT DOES NOT STOP	24	12	2.0	S			0.125	
27	85+09	66.4 LT	D3-1D	OLD SEWARD HIGHWAY	42	8	4.7	E/W	1-2.5" PT		0.125	
			D3-1D	34TH AVE	30	8	3.3	N/S			0.125	
			R1-1	STOP	36	36	9.0	W		0.125		
			W4-4B	ONCOMING TRAFFIC DOES NOT STOP	24	12	2.0	W		0.125		
28	85+28	27.6 RT	W1-7	RIGHT/LEFT ARROW	48	24	8.0	W	1-2.5" PT	0.125		
29	85+77	27.8 LT	R1-1	STOP	36	36	9.0	N	1-2.5" PT	0.125		
			R3-2	NO LEFT TURN	30	30	6.3	N			0.125	
			R3-1	NO RIGHT TURN	30	30	6.3	S			0.125	
			W4-4A	TRAFFIC FROM LEFT DOES NOT STOP	24	12	2.0	N			0.125	
					SIGN AREA SUBTOTAL		152.6					

FIELD BOOKS	BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REV	DATE	DESCRIPTION	BY
DESIGN: MOA Book 3576, Pages 1 - 73	GAAB 93	MOA 1988 Benchmark Book, D-54	113.95'	BASE	R&M		TELEPHONE	R&M					
STAKING:				TOPOGRAPHY	R&M		ELECTRIC	R&M					
ASBUILT:				PROFILE	R&M		CABLE TV	R&M					
CONTRACTOR:				SANITARY SEWER	R&M		TRAFFIC SIGNAL	R&M					
INSPECTOR:				STORM SEWER	R&M		DESIGN	USKH	WW				
CONSTRUCTION RECORD				WATER	R&M		QUANTITIES	USKH	WW				
				GAS	R&M		MUN. FINAL CHECK						

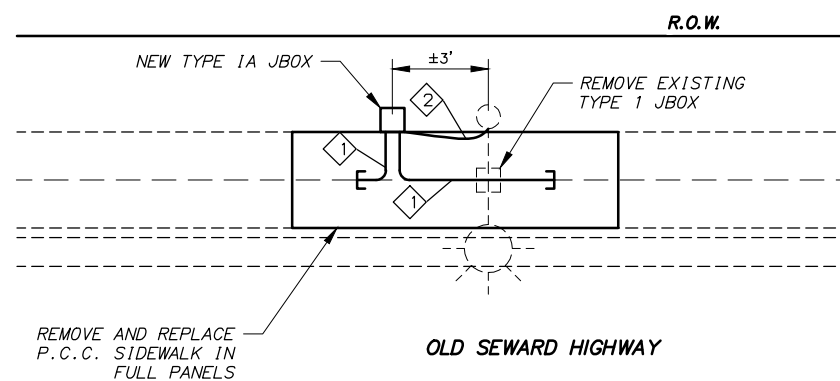
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STATE OF ALASKA
WILLIAM A. WEBB
1/4/2013
CE 12023
REGISTERED PROFESSIONAL ENGINEER



PUBLIC WORKS DEPARTMENT
10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH A
SIGN SUMAMRY
34TH AVENUE TO TUDOR ROAD
SCALE: N.T.S. DATE: OCT 2012 GRID: SW1631, SW1632 ACCT. NO. SHEET T11 of T14

FILE NO.



NOTES:

1. SAWCUT AND REMOVE SIDEWALK AT EXISTING JOINTS.
2. INSTALL NEW TYPE 1A JUNCTION BOXES 3 FEET SOUTH OF ELECTROLIER, AT THE BACK OF SIDEWALK, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
3. TOPSOIL AND SEED DISTURBED AREAS BEHIND THE SIDEWALK.
4. REMOVE EXISTING CONDUCTORS. PULL NEW CONDUCTORS TO NEXT ELECTROLIERS AND SPLICE TO EXISTING IN POLE BASE.

CONDUIT NOTES:

- ① 2" RMC
1-3c#6
- ② 2" RMC
2-3c#6

1 JUNCTION BOX REPLACEMENT DETAIL
SCALE: N.T.S.

FIELD BOOKS	BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REV	DATE	BY	REV	DATE	DESCRIPTION	BY
DESIGN: MOA Book 3576, Pages 1 - 73	GAAB 93	MOA 1988 Benchmark Book, D-54	113.95'	BASE	R&M		TELEPHONE	R&M								
STAKING:				TOPOGRAPHY	R&M		ELECTRIC	R&M								
ASBUILT:				PROFILE	R&M		CABLE TV	R&M								
CONTRACTOR:	BASIS OF DATUM: 1972 N.G.S. ADJUSTED DATUM			SANITARY SEWER	R&M		TRAFFIC SIGNAL	R&M								
INSPECTOR:				STORM SEWER	R&M		DESIGN	USKH	WW							
CONSTRUCTION RECORD				WATER	R&M		QUANTITIES	USKH	WW							
				GAS	R&M		MUN. FINAL CHECK									

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PUBLIC WORKS DEPARTMENT
10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH A.C.
DETAIL
34TH AVENUE TO TUDOR ROAD
SCALE: N.T.S. DATE: OCT 2012 GRID: SW1631, SW1632 ACCT. NO. SHEET T12 of T14

FILE NO.

DESIGN NOTES:

DESIGN: AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORT FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, 4th EDITION, 2001.

WIND LOAD: 100 mph w/ 1.3 GUST FACTOR

MATERIAL PROPERTIES		
CONCRETE	CLASS A	F'c=4 Ksi
REINFORCING STEEL	ASTM A615M	Fy=60 Ksi

BUS STOP ELECTROLIER FOUNDATION NOTES:

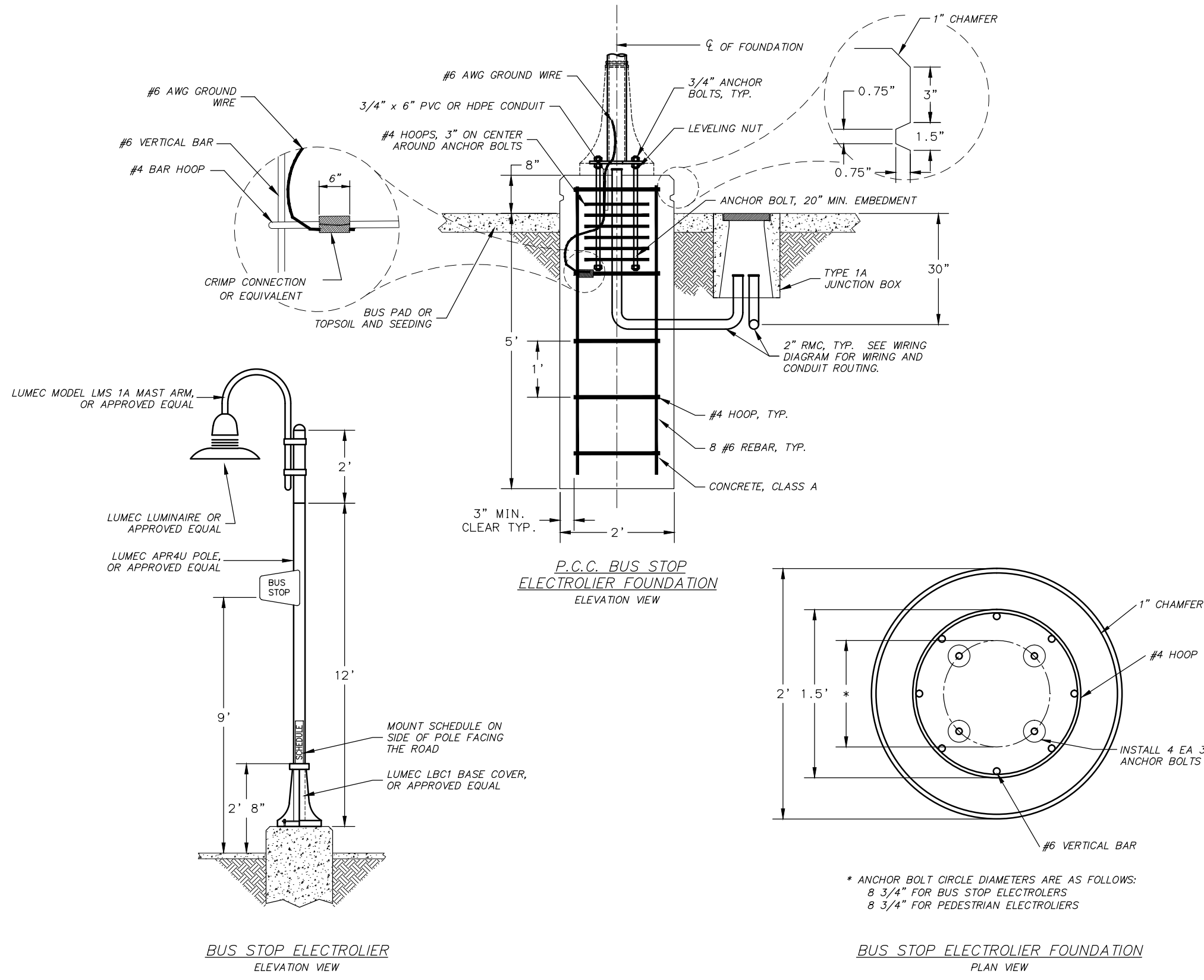
1. COMPLETE CONCRETE WORK IN CONFORMANCE WITH SECTIONS 30.01, 30.04, AND 80.04 OF M.A.S.S.
2. INSTALL 3/4" GALVANIZED ANCHOR BOLTS WITH A MINIMUM EMBEDMENT OF 20". ANCHOR BOLTS SHALL BE HOOKED OR HAVE A HEX NUT AND 5" Ø X 1/4" WASHER INSTALLED ON THE EMBEDDED END
3. FOUNDATION SHALL BE SLIGHTLY MOUNDED TO DRAIN AWAY FROM CENTER OF FOUNDATION.
4. CHAMFER EXPOSED CONCRETE EDGES 1".
5. BACKFILL AND COMPACT THE TOP 4" SPACE AROUND FORMS IN 8" LIFTS WITH A CEMENT-SOIL MIXTURE, CONSISTING OF 3 SACKS OF PORTLAND CEMENT PER CUBIC YARD OF SOIL.
6. TERMINATE THE 2" CONDUIT 1" ABOVE THE TOP OF FOUNDATION.
7. PROVIDE EXPOSED CONCRETE WITH A RUBBED FINISH.

GROUNDING NOTES:

1. USE #6 AWG GROUND CONNECTOR TO #4 HOOP REBAR WITH IRREVERSABLE CONNECTION 12 INCHES BELOW TOP OF FOUNDATION.
2. PROTECT GROUND WIRE WITH 3/4 INCH PVC OR HDPE CONDUIT TO 6 INCHES BELOW TOP OF FOUNDATION FILLED WITH SILICON SEALANT. FREE GROUND CONDUCTOR ABOVE TOP OF FOUNDATION TO 2 FEET.

LUMINAIRE NOTES:

1. PROVIDE A LUMEC DOMUS DMS50 LUMINAIRE WITH 65 WATT LIFELED, 4000° K LAMP THAT PROVIDES A FULL CUTOFF TYPE 3 LIGHT DISTRIBUTION. POLE, LUMINAIRE, AND MOUNTING ARM COLOR SHALL BE RAL 6016, TURQUOISE GREEN.



FIELD BOOKS	BM NO.	LOCATION	ELEV.	DATA	DRAWN BY	CHECKED BY	DATA	DRAWN BY	CHECKED BY	REV	DATE	BY	REV	DATE	DESCRIPTION	BY
DESIGN: MOA Book 3576, Pages 1 - 73	GAAB 93	MOA 1988 Benchmark Book, D-54	113.95'	BASE	R&M		TELEPHONE	R&M								
STAKING:				TOPOGRAPHY	R&M		ELECTRIC	R&M								
ASBUILT:				PROFILE	R&M		CABLE TV	R&M								
CONTRACTOR:				SANITARY SEWER	R&M		TRAFFIC SIGNAL	R&M								
INSPECTOR:				STORM SEWER	R&M		DESIGN	USKH	WW							
CONSTRUCTION RECORD				WATER	R&M		QUANTITIES	USKH	WW							
				GAS	R&M		MUN. FINAL CHECK									

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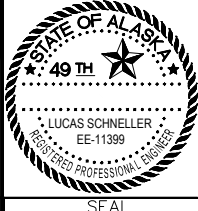
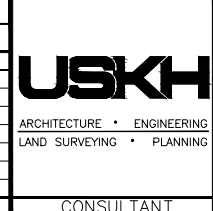
PUBLIC WORKS DEPARTMENT
10-13 OLD SEWARD HIGHWAY SURFACE REHABILITATION SCH C
BUS STOP ELECTROLIER DETAILS
34TH AVENUE TO TUDOR ROAD
SCALE: N.T.S. DATE: OCT 2012 GRID: SW1631, SW1632, SW1731, SW1732 SHEET T13 of T14

FILE NO.

NEW LOAD CENTER "B" SUMMARY, TYPE 2 INSTALLATION

LOAD CENTER LOCATION: STA. 64+36, 32' RT				SOURCE LOCATION: EXISTING ML&P PED.			
SERVICE: SINGLE PHASE, 3 WIRE, 120/240VAC 100A				SOCKET REQ'D. 4-JAW SAFETY SOCKET			
LOAD	MAIN BREAKERS			CONTACTORS			REMARKS
PANEL	240V	2P	60A				
PE CNTRL	240V	2P	15A	240V	2P	60A	
PANEL RATING: 10,000 AIC, 12 SPACE							
120/240 VAC 1-PHASE, 3 WIRE, 100A BUS							
CKT#	LOAD PANEL "A"			LOAD BREAKER			
	DESCRIPTION	KVA	AMPS	POLE			
A1	SOUTH LIGHTING	1.5	20	2			
A2	SOUTH LIGHTING 2	1.5	20	2			
A3	NORTH LIGHTING	2.6	20	2			
TOTAL LOAD KVA: 5.6							
* RUN CIRCUITS THROUGH CONTACTOR.							
NOTES:							
1. CONNECTED LOAD 5.6 KVA 23.3 AMPS; NEC DEMAND LOAD 7.0 KVA 29.2 AMPS							
2. PROVIDE LAMINATED LABELS, 1/4" BLACK LETTERS ON WHITE BACKGROUND, TO IDENTIFY LOAD AND DEVICE TAG AS APPLICABLE.							
3. PROVIDE CONTACTOR WITH 240V COIL, 0.1 KVA LOAD INCLUDED.							
4. MOUNT HOA SWITCH ON DEAD-FRONT INTERIOR COVER, WIRE FOR PE CONTROL IN AUTOMATIC POSITION.							
5. CONSTRUCT LOAD CENTER TO MASS DETAIL 80-46 (DIAGRAM E).							
6. ON SOUTH LIGHTING CIRCUIT, EVERY OTHER LIGHT SHALL BE ON ALTERNATING CIRCUITS.							

FIELD BOOKS										BM NO.		LOCATION		ELEV.		DATA		DRAWN BY		CHECKED BY		DATA		DRAWN BY		CHECKED BY		REV		DATE		BY		REV		DATE		DESCRIPTION		BY					
DESIGN: MOA Book 3576, Pages 1 - 73										GAAB 93		MOA 1988 Benchmark Book, D-54		113.95'		BASE		R&M		TELEPHONE		R&M																							
STAKING:														PROFILE		R&M		ELECTRIC		R&M																									
ASBUILT:														SANITARY SEWER		R&M		CABLE TV		R&M																									
CONTRACTOR:										BASIS OF DATUM: 1972 N.G.S. ADJUSTED DATUM				STORM SEWER		R&M		TRAFFIC SIGNAL		R&M																									
INSPECTOR:														WATER		R&M		DESIGN		USKH		WW																							
CONSTRUCTION RECORD												VERTICAL DATUM				GAS		R&M		MUN. FINAL CHECK		USKH		WW																					
												PLAN CHECK																																	



PUBLIC WORKS DEPARTMENT			
10-13	OLD SEWARD HIGHWAY SURFACE REHABILITATION	SCH C	
LOAD CENTER SUMMARY			
34TH AVENUE TO TUDOR ROAD			
SCALE: N.T.S.	DATE: OCT 2012	GRID: SW1631, SW1632, SW1731, SW1732	SHEET T14 of T14
ACCT. NO.			

FILE NO.